
F/YR21/1072/FDL

**Applicant: Mr A King
Ashwood Homes**

**Agent : Mr Nigel Lowe
Peter Humphrey Associates Ltd**

**Land East Of Bevills Close And North Of, Eastmoor Lane, Doddington,
Cambridgeshire**

Erect 47 x dwellings (2 x single-storey 2-bed, 11 x 2-storey 2-bed, 19 x 2-storey 3-bed, and 15 x 2-storey 4-bed), with associated garages, parking and landscaping, involving the demolition of existing agricultural building and garage to 44 Bevills Close

Officer recommendation: Grant subject to conditions and completion of a S106 legal agreement

Reason for Committee: Number of representations and Parish Council comments contrary to officer recommendation

1 EXECUTIVE SUMMARY

- 1.1 Doddington is a growth village (policy LP3 of the Fenland Local Plan) within the settlement hierarchy where small village extensions will be appropriate. LP3 does not define what small means, however policy LP4 states that large scale housing of 250 dwellings or more should be directed to market towns. Taking into account the shape and size of the village and the proposed 47 dwellings, it is considered that the scale of development and location of the site are acceptable in principle and accord with policies LP3, LP4 and LP12 of the Local Plan.
- 1.2 The key issues (other than principle) in relation to this site are access provision and drainage. The statutory consultees i.e. the Highway Authority and the Lead Local Flood Authority have raised no objections to the proposals subject to conditions. Similarly Anglian Water confirms that there is adequate capacity in the sewerage network to accommodate the development. The proposals demonstrate that adequate vehicular and pedestrian access is provided, and that surface water and foul sewage can be satisfactorily drained from the site. It is considered that in terms of highways and drainage issues, the proposal is acceptable and complies with policies LP12, LP14 and LP15 of the local plan.
- 1.3 With regard to other detailed matters including biodiversity, layout/appearance/design, impact on neighbour and future occupier amenity etc, the proposal is considered acceptable subject to conditions. In particular, the Council's Wildlife Officer comments that it will be easy to ensure that the site provides for a net gain in biodiversity.
- 1.4 The proposal is unable to meet the requirements for affordable housing set out in

local plan policy LP5 together with the infrastructure requirements to serve the development, which previously the local planning authority has accepted will be in the order of £2000 per dwelling (due to viability issues within the district). The applicant has submitted a viability assessment which concludes that the development can afford contributions to a value of £72,000. In lieu of this, a serviced land transfer has been negotiated which has an accepted value of £136,000. The likelihood of the school being able to obtain more land is low and so it is considered that this provision carries significant weight in favour of the scheme. The viability assessment must carry weight in favour of the scheme due to the acknowledged issues around viability within Fenland and because the Council's Senior Planning Obligations Officer has found this particular viability appraisal to be robust.

- 1.5 The latest Five-Year Housing Land Supply report (September 2021) shows that the Council currently has a 6.69 years supply of deliverable sites. The details of the supply can be found within the report which includes details of the calculations for the Local Housing Need figure. This indicates that 517 dwellings per annum will be required. An updated 5 Year Housing Land Supply report is due to be published in Autumn 2022 which will include calculations based on the most up-to-date available data. As such the tilted balance set out within the NPPF is not engaged. However, the existence of a 5-year supply is not reason to refuse acceptable residential developments coming forward. The NPPF sets out at paragraph 60 that the government's objective is to significantly boost the supply of homes. The lack of provision of affordable housing counts against the scheme but the provision of a mix of 47 market dwellings counts in favour of the scheme and balancing the two carries moderate weight in favour of the scheme.
- 1.6 In other respects, the proposal complies with the provisions of the development plan. It is acknowledged that the construction traffic and build programme generally will have a negative impact on nearby residents, but this is a temporary impact and not reason to refuse the application. This issue can only carry limited weight and can be tempered to degree by a CMP condition.
- 1.7 In conclusion, the application is considered to be acceptable subject to the imposition of conditions and the entering into of a S106 agreement to transfer the land for the school to the County Council.

2 SITE DESCRIPTION

- 2.1 The site comprises a roughly rectangular parcel of land currently in use for arable agriculture and measures approximately 3.12 hectares. It is situated to the east of residential properties off Harvest Close, Bevills Close and Eastalls Close. To the north of the site is Lionel Walden Primary School. To the east of the site are the properties located on the Manor Estate and to the south of this further agricultural land. To the south is Eastmoor Lane, which at this location is little more than a track and beyond that agricultural land.

- 2.2 To the east and north boundaries are drainage ditches and there is a strip of land which runs inside the majority of the west, south and east boundaries that forms a drainage easement and therefore cannot be built upon. There is an existing pond within the southern central area of the site. There are sporadic trees and hedging to the boundaries and various types of fencing where the site abuts the rear boundaries of adjacent dwellings. There is a row of trees along the north boundary with the school. There is no formal “made” vehicular access to the site at present except agricultural access off Eastmoor Lane. This lane contains a Public Right of Way which runs in a west/east direction crossing over the A141 and joining the wider network of footpaths in the countryside. The PROW also links through Eastalls Lane, terminating at St Mary’s Church.
- 2.3 The site lies within Flood Zone 1 which is the area at least risk of flooding. There is a sewage pumping station adjacent to the southeast corner of the site off Eastmoor Lane.
- 2.4 The Doddington Conservation Area lies to the northwest of the site and to the west, beyond Bevills Close. The northwest corner of the site abuts the corner of the Conservation Area boundary.

3 PROPOSAL

- 3.1 This is a full application to erect 47 x market dwellings (2 x single-storey 2-bed, 11 x 2-storey 2-bed, 19 x 2-storey 3-bed, and 15 x 2-storey 4-bed), with associated garages, parking and landscaping, involving the demolition of existing agricultural building and garage to 44 Bevills Close. The proposal provides for a single vehicular point of access via Bevills Close. The new road and footpaths would be situated off the existing stub road between 10 and 20 Bevills Close and would run to the south of the block of maisonettes comprising 12 – 18 evens Bevills Close. It would utilise existing highway land (the stub road) and an area of open space/landscape that is owned by Fenland District Council. Certificate B has been completed and notice served on the Council in this regard.
- 3.2 A cycle/footpath/emergency link is proposed to link into the site via the stub road and using part of the curtilage that currently comprises 44 Bevills Close (a bungalow in the ownership of the applicant). This bungalow shares a “joined” detached garage belonging to the owner of 39 Bevills Close, the neighbouring detached bungalow to No. 44 and situated to the north. In order to facilitate the new access, the part of the garage and the bungalow comprising No. 44 is proposed to be demolished and the new link would proceed where the garage now stands. The exposed wall of the remaining garage (belonging to No 39) would be made good. It is proposed to build a new 2 bed bungalow in place of the existing No. 44 property and provide two parking spaces within its remaining front garden. The proposed footpath/cycleway/emergency link would be a metalled surface of 3.75 metres in width. It would not be available for use by vehicles, and this would be achieved by placement of collapsible bollards at either end of the link and prohibitive signage (which would need to be agreed with the local highway authority as part of highway agreements). As in similar situations elsewhere, the emergency services would hold keys to the bollards.

- 3.3 Within the site, the new vehicular access enters from the west (as described above) and then turns north to form a central spine road from which the proposed dwellings are accessed directly or otherwise from a series of private drives. There is a hammer shaped turning head at the northern end of the spine road and a single spur at the southern end. The whole length of the spine road contains footways to either side.
- 3.4 Approximately one third of the site at the southern end is proposed to comprise public open space and will contain a rill and the existing pond with a separate further “dry” attenuation basin. This means that for the majority of the time this basin will remain dry and will become wet at times of heavy rainfall. The surface water would eventually discharge into the Anglian Water network at Eastmoor Lane. It is also proposed that this area of open space will contain an equipped children’s play area as well as soft landscaping. There will be a “hoggin” path/cycleway or similar linking from the new access south to join the PROW along Eastmoor Lane. It is proposed that this area of land will be managed by a private management company. The northern most part of the site is proposed to be conveyed to the primary school so that the school can extend its grounds. A footpath link is proposed to the northeast corner which will provide gated access to the rear of the school and a small hard surfaced pedestrian waiting area for parents, dropping off and picking up children. This area of land and pedestrian access would be managed by the school.

Full plans and associated documents for this application can be found at:
www.publicaccess.fenland.gov.uk/publicaccess/

4 SITE PLANNING HISTORY

F/YR07/1307/F – 44 dwellings refused 27.06.2008

F/YR14/0503/FDL – 66 dwellings refused 24.08.2015

F/YR16/0730/FDL – 62 dwellings refused 21.11.2016 for the following reasons (summarised);

- Village thresholds
- Lack of capacity at Doddington Waste Water facility for treatment of foul sewage
- Impact on amenity of existing residents from the proposed vehicular access and lack of alternative parking for occupiers of 12 – 18 Bevills Close
- Impact of noise from access on 12 – 18 Bevills Close and 15 Eastalls Close
- Development of 62 dwellings is not small scale and will have a cumulative detrimental effect on the neighbouring housing estate through vehicle movements into the village

5 CONSULTATIONS

5.1 Doddington Parish Council

The revised plans do not appear to help the vast number of objections that the original application has generated. Our main concerns are the access to the site being via the quiet and very narrow residential streets of the historic part of the

village – through Church Lane, Eastalls Close and Bevills Close. This issue has not been addressed nor has the issue of parking facilities for the residents of 12 – 18 Bevill’s Close. We reiterate our comments made at the last revision in May 2022.

*The Parish Council comments made on 19 May 2022 are as follows;
The analysis for both the site and access road shows very little clearance between passing vehicles and certainly does not address the issue of vehicles parked on the road which will make it impossible for large lorries to access the site meaning that the visibility and swept path are no longer valid. The new access to the site past the flats 12 – 18 Bevill’s Close means that those occupants will lose their existing parking area. The new access drawings also makes mention of “minor amendments” to 44 Bevill’s Close on one drawing, but a second drawing clearly shows it rebuilt in a completely different location. Demolishing 44 Bevill’s Close and rebuilding in a new location would surely further impact the viability of the development, which the developer already claims is marginal. The proposed development is totally unacceptable, and the revised proposals do nothing to address the concerns of the village.*

5.2 FDC Environmental Health

No objection in principle to the proposed scheme subject to further comments. The EHO team agree with the recommendations in the submitted Desk Study Report undertaken by Geodyne that a further ground investigation is necessary. This should include as a minimum a programme of exploratory hole undertaking and the subsequent testing of soil samples taken from the relevant parts of the site. This should be followed by a certified geotechnical analysis, the recommended asbestos survey and the installation of gas monitoring points and a programme of gas monitoring to be undertaken to determine the extent of gas presence and if gas protection measures will therefore be necessary. It is recommended this is required by pre-commencement condition along with the remaining suite of contaminated land conditions if planning permission is granted.

Consideration should be given to providing alternative sources of heating e.g. air source heat pumps, measures to promote sustainable travel and electric vehicle charging points. New developments can play a part in mitigating climate change and improving air quality. Use of ultra-low NOx boilers with NOx emissions of less than 0.04g/KWH of heat are more favourable than combined heat and power plant.

Given the nature of the proposal a Construction Management Plan (CMP) should be required and the EHO response sets out what sort of measures should be included in a CMP.

Officer comment: No comments made concerning proximity to pumping station

5.3 FDC Housing Strategy

Understand that a viability assessment has been submitted as part of the planning application and has demonstrated that it is not viable to provide affordable housing therefore no further comments to make.

5.4 FDC Planning Obligations Officer

Based on the evidence submitted and reviewed, I accept that the proposal is unable to deliver on site affordable housing but is able to provide a sum of £72,000 for S106 mitigation which delivers a profit level of 17.5% of GDV to the developer.

The value of the land proposed to be transferred to the education authority for the use by the primary school has been valued correctly. Agrees that the land to be offered up to the County Council has a higher value than the £72,000 accepted as the level of contribution the proposed development can afford.

5.5 FDC Estates

Part of the proposed access route is over public open space land owned by FDC. There is a restrictive covenant on the land which was transferred to FDC from Barratt Homes and it is our understanding that this remains the case. From an estates point of view, I have no view on the application, merely wished to highlight that the access could be problematical.

5.6 FDC Wildlife Officer

Responded that whilst the submitted Biodiversity Plan answers some of the points previously made further details will be needed. Any habitat creation methodology will need to demonstrate how the condition level described within the biodiversity metric 2.0 calculations will be achieved with on-going management. In addition, not all of the trees around the pond have been included within this plan, therefore it is unclear as to how the tree removals will impact the biodiversity net gain. The biodiversity metric has been updated since this submission so further information should be done using the Biodiversity Metric 3.1

The Wildlife Officer has clarified that it will be easy to achieve a biodiversity net gain on this site and that the further details required could be conditioned.

5.7 Highway Authority

Officer note: The comments below are based on the most recent plans which have been amended to taken into account (amongst other things) the original comments of the LHA. It is noted that the original comments made in October 2021 confirm that the officer responding visited the site.

With regard to the vehicle access arrangements around 12 – 18 Bevills Close, the submitted plans are acceptable with regard to carriageway and footpath widths and that forward visibility using a design speed of 20mph in accordance with Manual for Streets (MfS) is acceptable. The tracking plans show a refuse vehicle can negotiate the whole site including the left turn in and right turn out of Bevills Close. The tracking plans do not show two cars parking each other on the bends in this section. (Officer note: the applicant has now submitted a tracking plan showing that two cars can pass one another here).

Vehicle access in general – There is only a single point of access into the site via Eastalls Close and Bevills Close. Taking these two roads plus Walden Close means that there will be in the order of 131 dwellings (including the proposed site) served off a single point of access. An emergency access has been provided through plots 1, 2 and 44 to connect to Bevills Close. The emergency access should be moved slightly south of the footpath link and should follow the private access road alignment. (Officer note: The applicant has shown this on the most recent site plan). Collapsible bollards can be conditioned to ensure appropriate use of the emergency access. They can be secured using emergency locks – emergency services will have keys.

Parking with regard to 12 – 18 Bevills Close has been shown on the latest plans and is acceptable.

Footways have now been provided to the side of the access road adjacent to the public open space and on the south side through the S bend although the boundary treatment plan and site plan show a slightly different alignment. A 1.5 metre footpath link to the primary school is shown and will provide an overall shorter walk to the school. On road walking along Ingles Lane is far from ideal but a footpath cannot be provided here. The footpath link to the school should remain private.

The LHA agrees with the submitted transport statement with regard to trip generation and that these flows will not have a material impact on the highway network. The trip figures show that during the AM peak a total of 23 vehicles will arrive and depart; during the PM peak a total of 22 vehicles will arrive and depart and daily a total of 202 vehicles will arrive and depart.

Vehicle to vehicle and vehicle to pedestrian visibility splays are achievable and a 5.5-metre-wide continuous carriageway has now been provided. The parking spaces for plots 4 and 10 are a bit tight.

5.9 CCC Definitive Mapping Team

No objections. The developer must be aware that Public Footpath No. 6 runs to the south of the development site and must take account of its alignment and width.

5.10 CCC Archaeology

No objections in principle to development proceeding in this location. A Written Scheme of Investigation (WSI) has been submitted and agreed by CCC and if permission is granted, this WSI would need to be subject of a condition requiring that the investigations and reporting contained within the WSI are carried out.

5.11 CCC Education

The education authority is happy to accept the land at the north of the site in a serviced condition as it will provide more space for the primary school and that this would be in lieu of any other education contributions as it is understood that the

land is more valuable than the contributions would be and would enable better provision for the school.

(Officer comment: The County Council would be a party to the proposed legal agreement to secure the transfer of the land in question to the education authority)

5.12 Cambridgeshire Constabulary

The developer has been in consultation with the designing out crime officers and they have no particular concerns with this development. Has requested that a lighting plan be conditioned and that all private drives serving 4 or more dwellings are also lit with column rather than bollard lighting.

If cycle parking is by way of hoops, then they need to be cemented rather than bolted into the floor.

Entry to rear alleyways is limited and gates are shown. They should be as near to the house as possible and self-closing and lockable. The rear footpath to plot 15 should be gated at the fence line to the rear of plot 16 with a lock and self-closer, particularly as it is beside the waiting area and footpath for the school.

Queries the buffer zone to the rear of plots 6 – 16 i.e., who will manage it and who will be responsible for opening and closing the school gate? Comments that most burglaries are committed through rear gardens. The gate should only be open for a short time at times of school opening and closing.

Public footpaths should be straight, column lit and tree crowns raised to 2 metres to improve surveillance and to encourage residents' use of the green space which is a natural crime deterrent.

Overall, this layout this layout appears to be acceptable in relation to crime prevention and fear of crime.

5.13 Fire and Rescue Service

Fire hydrants should be secured through condition or S106 agreement

5.14 Lead Local Flood Authority (LLFA)

Based on the Flood Risk Assessment and Drainage Strategy, Inspire, AHL-1549 01-FRA-001-P3, July 2022, the LLFA has no objection in principle to the proposed development.

The above documents demonstrate that surface water from the proposed development can be managed through the use of a detention basin discharging to Anglian Water public surface water sewer and subsequent water course, restricting surface water discharge to 2l/s.

It is noted that some information has been omitted from the current application and request that a condition requiring this detailed information is imposed if planning permission is granted.

Consent from the relevant IDBs (Middle Level Commissioners and/or Curf and Wimblington Combined whose consents are managed by MLC will be needed for increased discharges into watercourses directly or indirectly or works affecting them will be required.

5.15 Anglian Water

The development is within 15 metres of a sewage pumping station. This asset cannot be practically relocated. AW consider that dwellings within 15 metres of this pumping station would place them at risk of nuisance in the form of odour, noise or general disruption from maintenance work. The layout should take this into account through a necessary cordon sanitaire e.g. open space or highway infrastructure to provide the development with a 15 metre separation from the boundary of the sewage pumping station.

The foul drainage from this development is in the catchment of Doddington Water Recycling Centre that will have available capacity for these flows.

The Drainage Strategy/SuDS proposals are acceptable to AW.

AW had previously commented that the development may lead to flooding downstream and suggests that the applicant work with the drainage authority to develop a feasible drainage strategy, however, it has since removed this comment from its latest response forwarded on 07/09/22.

(Officer note: The LLFA has accepted the submitted drainage strategy but requires a condition to be imposed which requires submission of further details. If approved, such a condition would be imposed and AW as well as the LLFA would be consulted).

5.16 East Cambs NHS

The change of use from agricultural to housing will impact on emergency ambulance services. A capital contribution may be required to help mitigate the ambulance service impact arising from the proposed development. The increase in demand for blue light services would generate a need for a contribution of £9,234 for the population arising from this development.

More people are likely to work at home as a result of the covid pandemic and the potential for community gardens/planting areas would be welcomed and would help support community physical and mental health and well-being.

Notes the development is in flood zone 1

Notes the transport statement and daily predicted trip rates of two-way vehicular flow of 202.

5.17 Cambridgeshire and Peterborough NHS

The development is likely to have an impact on the GP services operating in the vicinity of the application Fenland Group Practice: Doddington and Wimblington. The development will see an increase in patient pressure of 151 new residents which would require an additional GP/Nurse/admin support the increase in appointments and a need for 7.73 square metres net internal area. The model evidence from Cambridge Analytics shows that a contribution of £27,247.70 is required to mitigate the impact of the development.

5.18 Local Residents/Interested Parties

Objectors

A petition was sent to Cllr Connor, Chair of Planning Committee which set out that the access and traffic was the main issue of concern for most residents and that construction traffic would need to find another route. Also refers to the cul de sac leading to the proposed emergency access adjacent 44 Bevills Close, being a private road. There are 136 names on the petition.

At the time of writing, individual objections have been received from 166 households, the vast majority of which are in Doddington. The issues raised by objectors are summarised as follows;

- Access (see Traffic or Highways below)
- Density/over development – too many houses to close together
- Design/appearance
- Devaluing property – the people with views over the field bought their properties in good faith and this was reflected in the price. The development will impact on people's ability to sell their houses
- Loss of outlook/view – it will block the view of the field for all residents on that side of Bevills Close. It will be an eyesore from where my house is (1 Walden Close)
- Out of character with the area. The whole character of the area is being changed in the name of greed by selfish individuals who have no concern for current residents or future.
- **Traffic or highways.** Increase in traffic will be dangerous and noisy for those who live where it cuts through. The route to the site will be through narrow roads which will be a safety issue. Tweaking the plans here and there does not solve the problem. The site is still served by a single access off Church Lane and not only the increase in cars but also the construction traffic. Has anyone assessed how the construction traffic will enter the site? It doesn't matter how many times the plans are revised, the roads in Bevills Close and Church Lane simply cannot take the amount of traffic this proposal will generate. The traffic survey was carried out during covid so it is not realistic as most people were working from home. The High Street is already too busy in school times and this will make matters worse. Construction traffic should enter the site via a temporary entrance off the A141 and Eastmoor Lane. The footpath to the school makes it easier for school children but as the school is oversubscribed no children from this development will be able to attend. The swept path analysis shows little or no clearance between passing vehicles and doesn't take into account vehicles

parked on the road. The visibility splays at Bevills Close, Eastalls Close, Church Lane and Ingles Lane are entirely inadequate. Parts of Ingles Lane and Church Lane have no footpaths so pedestrians have to walk in the roadway. Church Lane is often lined with vehicles attending the church or Three Tuns pub as neither has its own parking. No mention of the narrow corner on Church Lane which would be the access to the site. These roads have no parking restrictions and are in poor condition. There is no traffic calming in the Church Lane area and speeding is prevalent and there is at present considerable risk of an accident. Each house will take at least 15 HGVs of materials to build and with groundworks that could mean over 2000 HGV vehicle movements going through narrow village roads. The majority of residents will have cars and the bus is unreliable. If this development is going to be considered, then the whole stretch of road between the 40mph and Wimblington should be considered. Also, a dedicated footpath/cycle path between Chatteris and March. Currently have to walk in the road from the corner at Primrose Hill. The traffic will pass my property (No 8 as it turns into the entrance road to the development and will be seriously impacted.

- It is an emotional view but why should two years of misery be imposed on us whilst the development is built?
- Visual impact. Doddington is an idyllic village with the area around the church being particularly enchanting. The development will destroy this part of the village. All the big developments in Chatteris and now Doddington will turn the area into March new town. The village will lose its appeal. Doddington village is being put at risk of losing its identity as a friendly quiet little village that has a strong close community. Fencing aims to create a gated community separate from the village
- Building unnecessary luxury houses on a greenfield site is wilful destruction of the environment
- Lack of affordable housing. New properties are supposedly to help first time buyers but these 3 to 4 bedroom houses will be beyond what most people can afford. They will be bought by wealthy people not solving the housing problem at all.
- Noise, air and light pollution. Noise from construction traffic
- Anti social behaviour- already a problem at the church from teenagers and this development will exacerbate this
- **Drainage.** If the surface water is made to move more slowly off the site (2 l/s) is there potential for it to back up and cause surface water flooding of the site? Contrary to comments from Anglian Water, the drainage can't cope and villagers have sewage and flood water in their gardens. They (Anglian Water) are always having to pump out the overflow down Eastmoor Lane. Anglian Water has acknowledged the plant down Conneywood Road is at capacity and is compensating by sending tankers to collect waste. Comments directed at Anglian Water concerning the duty to protect existing home owners/occupiers and the example of flooding in The Larches in Jan/Feb 2021 and the lack of interest from AW because it was surface water and not foul water and yet you comment on both matters in your response to this planning application. Since then a further 88 properties have been approved. The farmer has to leave the strip of land behind the existing houses unploughed due to the presence of the water pipe – surely this will be a problem for the developers. There are 6 large manholes along the rear of Bevills Close on the edge of the field that run from Eastmoor Lane to Harvest Close. Surely there has to be enough room left to

service these. Concern that surface water on the site will be displaced into neighbouring properties. One resident commented that she will not be seeking compensation from her insurance company but from FDC and Anglian Water if she is flooded and would encourage other residents to take the same approach until concerns are taken seriously and dealt with (this resident lives near to Juniper Close development)

- **Flooding.** Eastalls close flooded again as the drains can't cope. The Flood Risk Assessment is based on a broad scale and does not take into account local circumstances, in particular there have been recent flooding events which have been ignored (August 2014 – flooding caused stormwater drains to block and in December 2020 caused sewers to block. Both these led to flood water in my home (4 Church Lane) a grade II listed building. The development doesn't seem to take into account climate change and increasing rainfall
- Overlooking/loss of privacy
- Village cannot cope with more expansion. The school is oversubscribed, trying to see a GP at a local practice is challenging. The Council allowed the school car park to be built over in 1993 – don't make another mistake. Surely all of this site is needed to extend the school.
- Lack of facilities in the village – oversubscribed school and doctors. The development will generate approximately 23 children who would get priority due to proximity to the adjacent school but other children in Doddington will have to go elsewhere meaning more car journeys/traffic. Sending a four year old from Doddington to a school in Chatteris is ludicrous.
- Loss of agricultural land. Loss of food production land in Fenland. With what is happening in the world more land will be needed in this country for food production
- Parking arrangements – the proposal gets rid of the parking for the flats at 12 – 18 Bevills Close. Workers at the site are likely to park on surrounding streets.
- Proximity to property
- Residential amenity – loss of light to the rear of our property (15 Harvest Close). The houses would back onto me reducing privacy and open views. I am a keen astronomer, and the development would cause considerable light pollution (25 Manor Estate)
- Smell – already a smell because of the poor drainage
- Trees – use of Church Lane by heavy vehicles is likely to damage trees along that route
- Waste/litter
- Wildlife concerns. Although the plans depict trees this will not make up for the current flora and fauna supplied by the field to be built on. Bats are present in Harvest Close but they will disappear if this development goes ahead. Not sure if the proposals really compensate for the natural reserves on the site – particular reference made to birds. Could the green areas just be for wildlife? Could there be green roofs and wildflower areas? The development will drive more birds off the land and into gardens. The ecology report states the existing barn on site is used as a stopping point for a barn owl – shouldn't we be preserving their current hunting grounds rather than providing artificial owl boxes. The site is surrounded by sites identified by RSPB as potential breeding grounds for turtle doves.
- Would set a precedent
- Development has been refused 3 times already

- The human elements that make up the village will dwindle (neighbourly spirit of village). The identity of the place will be lost and places will be blended to make one mega town. It will bring more stress and unsettlement to the village
- Backfill
- Outside DAB
- The developer in latest published accounts (June 2021) acknowledged he is facing the impact of increased costs and supply issues of materials and labour following Brexit.
- The archaeological evaluation said the site is currently a lorry depot with buildings and a store but currently the land is used to grow crops
- This application has dragged on for such a long time and residents are looking forward to a conclusion
- Lack of security for school children using new entrance
- Policy LP12 refers to local support being required where development in Doddington exceeds 15% since April 2011. There is no clear evidence of support as the number of objections shows. Doddington has already exceeded its quota of housing set out in the local plan.
- Extension to the housing stock is not small scale as defined in policy LP3
- The current FDC Local Plans says that grown in homes and jobs will be closely linked to each other and that development will be appropriate and sensitive. This development falls short of these standards.
- 47 houses will increase the number of homes by c. 4.5 percent which is a huge increase and not sustainable.
- Conservation - Ingles Lane, Church Lane and part of Eastalls Close are all in Doddington Conservation Area and the church and 2 and 4 Ingles Lane are listed. Increased vibrations could cause damage over time to these walls and buildings.

Other comments

- The proposed wire fence around the development will give the appearance of a detention centre
- The development will make it very difficult for the occupier of 10 Bevills Close who will need to cross over the access road to get to his parking. The plans ignore numbers 8 and 10. The widened access will be nearer to the door of No. 10 and it is not clear if the footpath will remain on this side of the road.
- I live in one of the properties that backs onto the land that is proposed to be given to the school and will be enclosed by a weld mesh fence. This will be unsightly and will also mean that I have to erect a close boarded fence across the rear of my garden to maintain my privacy at my expense which is unacceptable. (comment from 11 Harvest Close)
- Reference made to central government's pledge to build beautiful homes in the right places and that the government was reconsidering its manifesto pledge to build 300,000 homes a year by the middle of this decade
- The developers have already suggested modifications to construction methods to reduce build costs – one wonders what other cost cutting will take place
- Question the demolition and rebuilding of 44 Bevills Close. This must be to increase access to the site for construction traffic before rebuilding it at the end of the project in a different place.
- I do have concerns but am not averse to the developers reconsidering their plans to be more accommodating

- The mobile phone signal is currently extremely poor and this will only get worse with more people trying to access it.
- There are some boarded up houses on the main road through the village. Why are these not being completed before any additional planning consent is granted?
- Electricity supply – the village has frequent power cuts. Even if heat source pumps are used, electricity will be the default supply and will be an unacceptable strain on an already unstable supply.
- Profit should never be put ahead of welfare and safety
- Why not move the school to Wimblington Road between the two villages and build on the old school property. This would result in removing a bottleneck in the village, more school places and sensible infill with adequate fall for the drainage
- Looks like the only problem this will resolve is the planners retirement fund
- The field itself is a valuable green area in the village and it should not be used to pack in more housing when surrounding towns have ample space for housing and the amenities to accommodate them.
- Safety concerns for children at school from the general public
- Children will not be able to play out in Bevills Close as they do now
- Two residents commented that no notification was received of the application
- The authorities seem to favour this application but who will speak up for the residents who have raised numerous objections?
- Cannot blame the public for thinking there are “back handers” and people being blindsided by the authority changing things after agreement is made
- Just reducing the number of properties by 20 from the last time the application was refused is not acceptable

- Occupier of 42 Bevills Close (in addition to many of the comments set out above)
- I am astonished that the highway authority find the plans acceptable and question the reasoning behind accepting this access. The only viable access is directly off the A141. I am led to believe that FDC must work with applicants to resolve problems but the unsolvable cannot be resolved. The access is not viable in real life, only on a drawing.
- I have never known such anger about an application and conversation is littered with words like “done deal” (officer comment: other similar phrases set out in response) and who can blame people for thinking that? The authority is helping the developer and not giving a damn about the local residents
- Regarding the lockable bollards at the emergency access in Bevills Close – these will be completely ineffective as motorcycles and scooters will avoid them. This problem exists between Bevills Close and Ingles Lane. The only effective barrier would be to clone the “access only” gate at the top end of Hospital Road, just off Benwick Road.

- Occupier of 15 Eastalls Close
- The development encroaches upon my property. My boundary goes up to the tree trunk and the fence was placed inside the tree to protect it.
- I live in the bungalow next to the proposed access. The disruption to me and my neighbours would be horrendous. Many of us are elderly. I am in poor health and I don't think I could cope with all the noise and my house being shaken by the large lorries

- This would set a precedent for all the large fields
- How is this helping our carbon footprint?
- Occupier of 39 Bevills Close (in addition to comments set out above)
- I live at 39 Bevills Close and my garage is attached to the garage that the developers plan to take down. Where will I stand if this happens? I will have a wall partly on someone else's property. Would the developer be willing to give me an extra couple of inches of land so the garage remains wholly on my land? The developer has not contacted me directly about this.

6 STATUTORY DUTY

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires a planning application to be determined in accordance with the Development Plan unless material planning considerations indicate otherwise. The Development Plan for the purposes of this application comprises the adopted Fenland Local Plan (2014).

7 POLICY FRAMEWORK

National Planning Policy Framework (NPPF)

Paragraph 11 – Presumption in favour of sustainable development
 Paragraphs 60 – 77 – Delivering a sufficient supply of homes
 Paragraphs 92 – 103 Promoting healthy and safe communities
 Paragraphs 104 – 113 Promoting sustainable transport
 Paragraphs 124 – 125 Achieving appropriate densities
 Paragraphs 126 – 136 Achieving well-designed places
 Paragraphs 159 – 169 Planning and flood risk
 Paragraphs 174 – 188 Conserving and enhancing the natural environment

National Planning Practice Guidance (NPPG)

Viability and decision taking – applicant to provide evidence and it is for the decision maker, having regard to all the circumstances in the case to decide how much weight to give the viability assessment.

Use of conditions including Circular 11/95 Model Conditions

National Design Guide 2021

Fenland Local Plan 2014

LP1 – A Presumption in Favour of Sustainable Development
 LP2 – Facilitating Health and Wellbeing of Fenland Residents
 LP3 – Spatial Strategy, the Settlement Hierarchy and the Countryside
 LP4 – Housing
 LP5 – Meeting Housing Need
 LP12 – Rural Areas Development Policy
 LP13 – Supporting and Managing the Impact of a Growing District

LP14 – Responding to Climate Change and Managing the Risk of Flooding in Fenland

LP15 – Facilitating the Creation of a More Sustainable Transport Network in Fenland

LP16 – Delivering and Protecting High Quality Environments across the District

LP17 – Community Safety

LP19 – The Natural Environment

Emerging Local Plan

The Draft Fenland Local Plan (2022) was published for consultation on 25th August 2022, the first stage of the statutory process leading towards the adoption of the Plan. Given the very early stage which the Plan is therefore at, it is considered, in accordance with Paragraph 48 of the NPPF, that the policies of this should carry extremely limited weight in decision making. Of relevance to this application are policies:

LP1, LP2, LP7, LP8, LP12, LP18, LP20, LP22, LP24, LP27, LP28, LP31, LP32

8 KEY ISSUES

- **Principle of Development**
- **Access, highway safety, parking**
- **Appearance, Character, Layout and Design**
- **Biodiversity**
- **Drainage/Flood Risk**
- **Neighbour amenity (future occupier amenity)**
- **Infrastructure and viability**
- **Other Issues**
- **Planning balance and conclusions**

9 BACKGROUND

See site history set out above

10 ASSESSMENT

Principle of Development

- 10.1 Doddington is identified as a growth village within the spatial strategy and settlement hierarchy for Fenland as set out in policy LP3 of the Fenland Local Plan 2014 (the local plan). The policy states that for growth villages, development and new service provision either within the existing urban area or as small village extensions will be appropriate, albeit at a considerably more limited scale than that appropriate to market towns.
- 10.2 The policy contains a note which states that for development in Wimblington and Doddington, development will be appropriate provided that capacity at or in the sewerage network leading to the wastewater treatment work at Doddington can be addressed. Developers are advised to contact Anglian Water.

- 10.3 Policy LP4 sets out the housing targets for the district and the plan enables 11,000 new homes to be built in the district between 2011 and 2031. For development in and around villages, policy LP4 points to policies LP3 and LP12 for criteria to assess proposals for housing. Large scale housing i.e., 250 dwellings or more is directed to specific and broad locations around the market towns. Any other large-scale housing on the edge of market towns or away from these areas will be refused.
- 10.4 Policy LP12 Part A sets out that for villages, new development will be supported where it contributes to the sustainability of that settlement and does not harm the wide-open character of the countryside. Any proposal will need to satisfy the applicable policies of the plan, including policy LP3 (settlement hierarchy) and the following criteria;
- (a) The is in or adjacent to the existing developed footprint of the village; and
 - (b) It would not result in coalescence with any neighbouring village; and
 - (c) It would not have an adverse impact on the character and appearance of the surrounding countryside and farmland
 - (d) The proposal is of a scale and in a location that is in keeping with the core shape and form of the settlement, and will not adversely harm its character and appearance; and
 - (e) It would not extend linear features of the settlement or result in ribbon development; and
 - (f) The site retains and respects natural boundaries such as trees, hedgerows, embankments and drainage ditches; and
 - (g) The site retains and respects ecological, heritage and biodiversity features; and
 - (h) It would not result in the loss of important open space within the village; and
 - (i) It would not result in the loss of high-grade agricultural land, or if so, comprehensive evidence is provided to justify the loss. This should include an assessment of all alternative reasonable opportunities in the locality to develop on lower grades of agricultural land; and
 - (j) It would not put people or property in danger from identified risks; and
 - (k) It can be served by sustainable infrastructure provision, such as surface water and wastewater drainage and highways.
- 10.5 Doddington is a growth village, therefore is one of the second-tier settlements within the district after the market towns where development within or adjacent to the built-up area will be appropriate, albeit at considerably more limited scale than in the market towns. Whether development comprising 47 dwellings is an appropriate scale is somewhat subjective. However, policy LP4 identifies large scale proposals as being 250 plus dwellings and sets out that these are only appropriate in the identified locations at market towns. Compared to the scale of development sites being proposed at the market towns, this site is relatively modest and is of a considerably more limited scale than that appropriate to market towns.
- 10.6 In terms of location, the site its partially within and certainly adjacent to the developed footprint of the village and so is compliant with parts (a) and (b) of policy LP12. It is immediately to the east of the built-up area comprising Bevills Close, Eastalls Close and Harvest Close. The northern part of the site is immediately to

the west of the built-up area of the Manor Estate and the north boundary lies adjacent to the south of the primary school. The shape and general density of the development and the size of the site is akin to the Bevills Close development and cannot be said to be out of keeping with the core shape of the village, nor does it extend linear features or create ribbon development. The proposal would also not create coalescence with a neighbouring village. In this regard the development is compliant with parts (b), (d) and (e) of policy LP12.

- 10.7 Part A of Policy LP12 states that if a proposal within or on the edge of a village would, in combination with other development built since 2011 and committed to be built, increase the number of dwellings in a growth village by more than 15%, the scheme should have demonstrable evidence of clear local community support. The appeal decision for application F/YR14/0838/O relating to a residential development in Manea provided the following comments from the Inspector, who in allowing the appeal stated *“the Council’s objection to the scheme is based solely on the lack of clear community support. However, it is highly unusual for any new housing scheme to be supported by the local community particularly where it would involve the loss of open land as is the case here. The Planning Practice Guidance advises that planning is concerned with land use in the public interest and that the level of local objection is not in itself a reason to withhold planning permission.”* Since this criticism of this aspect of policy LP12, the local planning authority has not sought to engage this part of the policy.
- 10.8 It is therefore considered that the principle of residential development at this location is acceptable in principle. The other parts of policy LP12 fall to be considered within the following sections of this report and alongside the other relevant policies within the local plan. Whilst little weight can be attributed to this currently, the site is shown as being outside the settlement boundary for Doddington within the emerging local plan.

Access, highway safety and parking

- 10.9 The proposed vehicular access to the site is to be taken off Bevills Close, between Nos. 10 and 20 and adjacent to the block of 4 maisonettes comprising 12 – 18 Bevills Close. The area of highway here presently comprises a rectangular shaped turning area with footpaths to all sides. The proposals would see the majority of this turning area become an access road into the site, passing through the existing landscape area to the south of the maisonettes before entering the site. Within the site the access road comprises a 5.5-metre-wide carriageway with 2 metre footpaths to both sides. Dwellings are served either directly off this road or off private drives.
- 10.10 A second access is proposed further north and is to be taken off Bevills Close in the location of an existing bungalow, No. 44. This access comprises a 3.75-metre-wide shared footpath cycleway and would also serve as an emergency access. It would not serve as a vehicular access to the site and would have lockable bollards at either end of the link to prevent this. In order to accommodate this access, the bungalow at No. 44 is to be demolished and a new bungalow rebuilt in a different position within the plot. The part of the joint garage shared with No. 39 will also be removed and the shared wall made good resulting in this garage becoming a single

detached garage for No. 39. A further footpath/cycleway is proposed to link the development through to Eastmoor Lane.

10.11 Bevills Close is reached from Eastalls Close which in turn is accessed from Church Lane. Church Lane has three entry/exit points off High Street and New Street. The entrance from High Street is via Ingles Lane which varies in width, as does Church Lane and in part contains no footpaths. St Marys Church (grade II listed) lies at the corner of Church Lane. This area comprises the historic core of the village and forms part of Doddington Conservation Area.

10.12 Concerns about the access to the site and the route leading to it through the historic core of the village is one of the key concerns raised by the objectors, as well as its impact upon existing residents. These comments are summarised in the above neighbour comments section of the report.

10.13 Policy LP15 (C) requires all development proposals to demonstrate they have had regard to the following criteria;

- Development on a site should be located and designed so it can maximise accessibility and help to increase non-car modes
- Proposals which include new public highway should ensure such new highway complements the character of the area
- Proposals for one dwelling or more should be accompanied by a Transport Statement, Transport Assessment and Travel Plan (commensurate with the scale of the development)
- Development schemes should provide well designed safe and convenient access for all, give priority to the needs of pedestrians, cyclists, people with impaired mobility and users of public transport by providing a network of pedestrian and cycle routes and green corridors which give easy access and permeability to adjacent areas
- Development schemes should provide well designed car and cycle parking appropriate to the amount of development proposed and in accordance with the Council's defined parking standards set out in Appendix A (to the local plan)

10.14 A Transport Statement was submitted with the application. This statement sets out the predicted traffic to be generated from the proposed development once completed. The statutory consultee with regard to highway matters is the Local Highway Authority. The initial comments provided by the LHA raised no concerns with regard to the location of the proposed vehicular access but did raise detailed comments about adjustments and amendments to the road alignment and requirement for tracking plans. The LHA commented that the proposed vehicular access was the single point of access to a development that would lead to in the order of 131 dwellings being served off Eastalls Close and Bevills Close and as such a second point of access would be envisaged. It was requested that a second point of access (which could take the form of an emergency access) be provided. The proposed cycle footpath from Eastmoor Lane was suggested as a potential vehicle access. No comments were received from the LHA or have been received since which question the sufficiency of the local roads to take the additional traffic and indeed in response to the Transport Statement, the LHA agreed with the findings of the statement and concluded that traffic flows would not have a material impact on the highway network. The revised plans show the

emergency access with bollards being provided via the new footpath/cycle route through Bevills Close (No. 44). The highway authority has not objected to this being the required emergency access. The case officer has checked with Cambridgeshire County Council as to whether this part of Bevills Close is adopted or private road as this has been raised by residents. The County Council has confirmed and provided evidence of the adoption agreement for this part of Bevills Close which confirms that it is highway land.

- 10.15 The LHA comments also noted the narrow parts of Ingles Lane and lack of footpath and asked if consideration had been given to a direct link into the school as the site shares a border with the school. The revised plan shows a direct footpath link to the school from the development, and this has been accepted by the County Education Authority. The access point to the school via this footpath would be managed by the school and the land would be part of the parcel “given over” to the County Council via the proposed legal agreement.
- 10.16 The LHA comments also noted the previous reason for refusal concerned with lack of parking for 12 – 18 Bevills Close. The access road removes the alignment of the square. The LHA comments “*while this has been used for parking this is not allocated parking because allocated parking cannot be provided on the public highway. 12 – 18 appears to have their own off street parking area.*” The maisonettes do have allocated off road parking adjacent to this part of the highway. In agreement with the comments made by the LHA, although parking may occur on the highway, this does not mean that the highway constitutes a parking area.
- 10.17 Other comments referred to detailed design elements of the access roads and footways etc. No comments were made about construction traffic.
- 10.18 Since then, several iterations of the plans have been received and the latest comments from the LHA are set out in the Consultee Comments section of the report. Of note is that the proposed footpath into the site via 44 Bevills Close has been “upgraded” to provide the emergency access required by the LHA who have not objected to this in principle.
- 10.19 As the LHA has not objected to the proposed access points nor the basic design of the accesses at these locations, the LPA must have due regard to this advice and consider that the proposed access points will provide safe access to serve the development. The case officer does not doubt that the local change to these areas in Bevills Close will be keenly felt particularly by near neighbours, but this does not mean that the proposed accesses do not come up to standard and indeed the developer proposes to put the access forward for adoption by the local highway authority.
- 10.20 Access to the site for construction traffic is difficult and will mean that vehicles attending the site will need to travel along Church Lane and Eastalls Close prior to entering Bevills Close. The agent for the developer was asked about this and responded “*we did look at alternative access routes and unfortunately the key part of Eastmoor Lane (which connects to the A141) is privately owned and not suitable for delivery vehicles. We regularly deal with the management of deliveries in built up areas and employ a materials co-ordinator to schedule deliveries, agree routes, timings, logistical site storage etc, together with any necessary road cleaning and*

liaison with residents. All deliveries to the Doddington site would be carefully coordinated, with off-site holding areas identified as required (public laybys, lorry parks etc – not neighbouring streets). Obviously, no development is completely without some inconvenience to local residents...”

- 10.21 The local planning authority regularly imposes a condition on larger developments to require submission of a Construction Management Plan (CMP). It would be possible to include within such plan hours of working, times of deliveries, dust management, provision of parking for workers on site, materials store areas on site etc. Circular 11/95 sets out advice on use of conditions. Paragraph 71 of this document states that planning conditions are not an appropriate means of controlling the right of passage over public highways. It states that it may be possible to encourage drivers to follow preferred routes by posting site notices to that effect or by requiring them to use a particular entrance or exit from the site, but where it is essential to prevent traffic from using particular routes, the correct mechanism for doing so is an Order under either section 1 or 6 of the Road Traffic Regulation Act 1984.
- 10.22 Construction traffic and construction in general is disruptive by nature. Development often takes place in close proximity to other occupiers. However, it is of a temporary nature and can be managed to a degree by a robust CMP. It is therefore proposed that if planning permission is approved that it is subject to a CMP but that this cannot include controls outside of the site area or control over the public highway. The CMP should also contain the contact details for the site manager or person on site responsible for implementing the CMP and out of hours contact details so that members of the public can contact the developer directly should problems arise. It is very unlikely that a reason for refusal based upon disruption caused by construction traffic would stand up to scrutiny on appeal. It is noted that the Council's Environmental Health team has not raised this as an issue.
- 10.23 Appendix A to the local plan sets out the Council's car parking standards. Dwellings up to three bedrooms require 2 parking spaces. Dwellings of four bedrooms or more require 3 parking spaces. Garages can be counted as a parking space provided the internal measurement meets or exceeds 7.0 x 3.0 metres internally. Appendix A does not set out minimum sizes for parking spaces but the usual practice of the LPA is to seek spaces measuring 3.0 metres wide by in the order of 6.0 metres in length. The parking spaces proposed are generally 2.75 metres in width by a varying length. The majority are at least 5.5 metres in length. The spaces serving plots 15, 16, 17 and 19 are shown as 5.0 metres in length but the width of the private drive at this location (ie 7.0 metres) provides some scope for overhang if necessary. In many instances the driveways enable some flexibility as they are slightly longer than the spaces as marked out on the submitted site plan. The LHA highlighted two plots where it considered the parking a bit "tight" but is now satisfied that the parking arrangement for plots 4 and 10 has been improved. Each plot has the correct number of spaces for the size of dwelling proposed. The proposed garages do not meet the specifications set out in Appendix A to be counted as parking spaces, except the double garages can be counted as one space. Nevertheless, for plots that have garages, there are sufficient on drive parking spaces to meet the requirements set out in Appendix A. Whilst there is some frontage parking, much of the parking is set back between the

dwellings so that the street scene is not dominated by parking. Overall, it is considered that the parking layout and provision for each plot is acceptable.

- 10.24 With regard to sustainability, the site is in a sustainable location in relation to the centre of Doddington. Apart from the road access, the footpath/cycleway access via the proposed emergency access provides a link through to Bevills Close and then in turn through the existing footpath to the north of Bevills Close onto Church Lane. The footpath through the open space will link to Eastmoor Lane which is a public right of way. The footpath link through to the school will provide a safe route to the school from the site and from Bevills Close.
- 10.25 In summary, the LHA has confirmed that the traffic generated by the development once complete, will not have a material impact on the highway network. It has not raised any issues of highway safety. The design of the access road and emergency link are accepted in principle and would be subject to further detailed design which can be conditioned. The site provides footpath/cycle links in addition to the main access and also provides a new pedestrian link to the school. The route for construction traffic is not easy but this is temporary and not all sites can be accessed directly from a main road. The imposition of a condition to require a CMP will help reduce the impact to residents and no working or deliveries will take place at unsociable hours. It is therefore considered that the proposal is acceptable in highway terms and complies with policy LP15 of the Local Plan. It must be noted that paragraph 111 of the NPPF states that development should only be prevented or refused on highway grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Appearance, character, layout and design

- 10.26 Policy LP16 of the local plan states that high quality environments will be delivered and protected throughout the district and that all new development will only be permitted where it meets criteria set out within the policy. In terms of layout and design, criteria (d) requires that development makes a positive contribution to local distinctiveness and character of an area, enhancing its local setting, responds to and improves the character of the local built environment, provides resilience to climate change, reinforces local identity and does not adversely impact, either in scale or design terms, on the street scene, settlement pattern and landscape character of the surrounding area. Criteria (i) requires provision of well-designed hard and soft landscaping incorporating sustainable drainage systems as appropriate. Criteria c), d) and e) of policy LP12 require developments to be appropriate for the character and appearance of the area, in scale and in a location that is in keeping with the core shape and form of the settlement and do not extend linear features/create ribbon development.
- 10.27 Paragraph 174(b) of the NPPF states that policies and decisions should recognise the intrinsic character and beauty of the countryside and wider benefits from natural capital and ecosystem services – including the economic benefits of best and most versatile land and of trees and woodland. Paragraph 130 of the NPPF sets out a series of criteria for achieving good design – planning policies and decisions should function well and add to the quality of the area, are attractive as a result of good architecture and landscaping, sympathetic to local character and

history, establish a strong sense of place, using arrangement of streets, spaces building types and materials to create attractive places, optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public open space) and support local facilities and transport networks and create places that are safe, inclusive and accessible with a high standard of amenity for existing and future users and where crime and fear of crime does not undermine the quality of life.

- 10.28 The site comprises “green field” land abutting the settlement but is considered to be countryside therefore it has value for its own sake i.e., its intrinsic character and beauty and the wider benefits it brings. The site is also closely related to Bevills Close to the west, Manor Estate to the east and the school to the north. It comprises generally flat arable farmland with hedges and trees to the borders particularly to the south and southeast boundaries and it contains a stand of trees adjacent to the pond within the southern portion of the site. The proposed area for the vehicular access presently comprises a small area of open space with trees. Otherwise, the site is devoid of features.
- 10.29 The site is of a similar shape and size to Bevills Close, which from the records the LPA holds appears to have been built in the late 1970’s/early 1980’s. The Manor Estate development appears to date from a similar period. The Bevills Close estate separates the proposal site from the historic core of Doddington and the Conservation Area. Only the very top northwest corner of the site abuts the corner of the Conservation Area. Bevills Close comprises a mixture of two storey and single storey dwellings and contains 44 dwellings. The proposed development comprises two storey detached, semi-detached and some terraced dwellings and two bungalows. Aside from fewer bungalows, the layout and overall density of development is not too dissimilar to that in Bevills Close. The properties in Bevills Close, Harvest Close and Walden Close contain various plot sizes and also some gardens that are irregularly shaped. Again, this is not too dissimilar to the layout being proposed. The proposal provides for a layout that is of a similar character to that of the adjacent development and it is therefore considered that it is in keeping with the character of the adjacent and most comparable layout and density of development.
- 10.30 The proposed dwellings “address” the main spine road i.e. the front elevations face the street. There are no instances where only a side elevation faces the main street. At key corners, the Humber house type is used which has a two-storey outshot from the main part of the dwelling so providing an L shape which follows the corner around. The outshot does contain two windows at ground floor and a small window at first floor, thereby not presenting an entirely blank elevation to the street. Each dwelling has space to the front in the form of a small front garden area, including the smaller terraced type properties at the northern end of the site. The submitted materials schedule and plan shows four proposed brick types – a red, red multi, cream and cream multi and a mix of concrete plain grey tiles and two different pantiles. Properties on the adjacent development comprise a mix of red and buff/cream bricks, rendered properties (mainly the bungalows) and brown/red concrete tiles. The proposed materials are not out of keeping with the existing properties on Bevills, Eastalls land Walden Closes.

10.31 Any development on a greenfield site will inevitably result in the change of the openness of that site. In this case, the site is surrounded on 2.5 sides by built development. The site itself is relatively flat and devoid of features except for the existing pond and stand of trees to the southern end. It is an arable field. The countryside to the east and to the south beyond Eastmoor Lane is similar arable land comprising even larger fields, is flat and devoid of features except for small amounts of boundary hedgerow. There are no public rights of way along the edges or within the site except for the right of way along Eastmoor Lane to the south. It is considered that there are no particular features nor landscape character that makes this site stand out from surrounding farmland that warrants its retention for its own sake. Therefore, in terms of character and appearance, the proposal is considered to be acceptable, subject to conditions, and is in compliance with policy LP16, particularly criteria d and i in this regard and the relevant criteria of policy LP12.

Biodiversity

- 10.32 Policy LP16 requires that development protects and enhances biodiversity on and surrounding the proposal site. Policy LP19 requires the Council to work in partnership with relevant stakeholders to conserve, enhance and promote biodiversity in the interest of the natural environment in Fenland. It requires particular protections for priority habitats and species and that opportunities be taken to incorporate beneficial features for biodiversity in new developments.
- 10.33 Paragraph 180 of the NPPF states that where significant harm to biodiversity cannot be avoided or adequately mitigated or as a last resort compensated for, then planning permission should be refused. Paragraph 179 states that opportunities should be sought for securing a measurable net gain in biodiversity.
- 10.34 The submission includes an Extended Ecological Survey and Addendum. This sets out that the building on site is used as an occasional bat roost and by a barn owl. The survey of the building concluded negligible potential for bat roost with no evidence of a bat roost and absence of suitable roosting features. The site offers suitable foraging and commuting habitat for bats. Suitable habitat exists on surrounding land for this purpose and for nesting birds. There was no evidence of badger setts or reptiles. Recommendations include 25% of properties being constructed with bird boxes, provision of bat boxes, a barn owl box to a retained tree and hedgehog friendly fencing. In terms of biodiversity enhancement, the ecological consultant for the developer considers that an improvement by 10 percent is achievable. The development will result in some tree loss – to the public open space where the main access is to be located and removal of trees near the existing pond. However, there is adequate land to provide for new tree planting to compensate for this loss as well as providing enhancements. The trees at the borders of the site are being retained.
- 10.35 The Council's Wildlife Officer has stated that a biodiversity net gain should be relatively easy to achieve on this site and that further detail with evidence of an up-to-date measurement of the biodiversity should be conditioned through submission with a landscaping scheme and associated Ecological Design Strategy.

10.36 The site does not contain priority habitats or wildlife designations. Due to the arable and open nature of the site, i.e. lack of many features, the development will not result in significant harm to biodiversity. There will be some loss, but this is capable of being compensated for and enhanced and this can be achieved through further submissions. The loss has to be seen in context of the arable nature of the site and the vast areas of open countryside that remain especially to the east and south of the site. The development therefore complies with the NPPF and relevant parts of policies LP16 and LP19 of the Local Plan.

Drainage/Flood Risk

10.37 The site lies within Flood Zone 1 which is land at least risk of flooding. The site (northern part) is partially within a large area of groundwater vulnerability that covers the majority of the northern part of Doddington.

10.38 The proposed foul drainage strategy is to provide a sewer pipe with manholes along the main spine road. The land naturally falls slightly towards the south. Each dwelling will connect to the main foul sewer and this in turn will travel south, across the proposed open space and connect with the existing Anglian foul network at Eastmoor Lane. The surface water strategy is to provide permeable paving to the private drives and a 600 diameter surface water drain along the main spine road. This will carry surface water to a filter drain strip located along the southern most edge of the access road. Surface water will be attenuated to a greenfield run off rate at 2 litres per second and will discharge to the Anglian Water network on Eastmoor Lane. The existing pond is to be retained within the public open space and a dry basin with an ability to store 1140 cubic metres of surface water in times of heavy rainfall will be provided. Most of the time this basin will be dry. This will also connect to a hydrobrake manhole at the southwest corner of the site where the water will discharge at a rate of 2 litres per second into the existing network.

10.39 Many of the people who have raised objections have referred to existing drainage issues in Doddington including flooding or waterlogging in gardens (especially at times of high rainfall), need to have sewage pipes pumped out regularly by Anglian Water and issues of smell. The concern is that the proposal will make the situation worse.

10.40 The LLFA is the statutory consultee on issues of flood risk and surface water drainage and does sometimes comment on foul drainage where this might impact flood risk or surface water. Anglian Water is consulted as the drainage undertaker who provides infrastructure for carrying and dealing with surface water run-off and sewerage. The Internal Drainage Board is consulted where it manages infrastructure such as drainage ditches, that might be impacted by proposals.

10.41 Anglian Water has stated that the Doddington Water Recycling Centre does have capacity to deal with sewerage from the proposed development. It states that it accepts the drainage strategy for surface water and foul drainage (which is that both will drain to existing AW network). The developer would need to apply to Anglian Water to connect to its apparatus. The LLFA has accepted the surface

water drainage strategy but requires further details concerning its design which can be conditioned.

- 10.42 Policy LP12 requires new development to be served by sustainable infrastructure such as surface water and wastewater. Policy LP14 requires all development to adopt a sequential approach to flood risk and to use Sustainable Drainage Systems to ensure that surface water run-off is to green field run off rates.
- 10.43 As the site lies within flood zone 1, there is no requirement to apply the sequential test to this proposal. Neither the LLFA nor Anglian Water have objected to the most recent drainage strategies which show that surface water run-off can be attenuated to a run-off rate of 2 litres per second. Anglian Water has its own consenting regime which the developer would need to follow, should planning permission be granted. The LLFA require details of the surface drainage strategy to be submitted prior to any laying of services, hard surfaces or erection of a building takes place. The case officer considers this condition to be appropriate.
- 10.44 The application has shown that the proposed drainage is a sustainable drainage system. Neither the LLFA nor AW has objected to the proposals. The application complies with the above policies of the local plan subject to further details being submitted. Given that this is the case, the application cannot be recommended for refusal for drainage issues.

Neighbour Amenity (future occupier amenity)

- 10.45 Policy LP16 of the local plan requires that development does not adversely impact on the amenity of neighbouring users in areas such as noise, light pollution, loss of privacy and loss of light.
- 10.46 The proposed dwellings situated nearest to the west boundary of the site adjoining neighbouring rear gardens, have been positioned so that the side elevations face the rear of the neighbouring dwellings. The only exceptions are proposed plots 37 and 38, however, these dwellings are set in by 19 metres from the west boundary of the site and there is a 46-metre separation distance between the dwellings (20 and 22 Bevills Close and plots 37 and 38). There is a 25 metre separation distance from the rear of the flats at 14 and 16 Bevills Close and the rear of the proposed dwelling at plot 37 and additionally these buildings are slightly off set. Where there are side elevations facing properties in Bevills Close, the proposed dwellings are set back from the site boundary due to the need to maintain the drainage easement along the boundary but within the site. A similar relationship occurs where the side of proposed dwellings faces the rear of dwellings on Manor Estate. Notably the gardens to the existing properties here are very long. This results in there being no issue of overlooking or overshadowing to existing dwellings as a result of the scale and positioning of the new dwellings.
- 10.47 In general terms the positioning of residential development adjacent to existing residential development is not going to raise issues of noise nuisance or light pollution because the uses are compatible. In other words, the proposed use is not employment or other such development where noise, once constructed could continue to be an issue. No doubt the actual construction works will be disruptive as construction works are, but they will only take place at reasonable hours and

can be tempered to a certain degree by the imposition of a Construction Management Plan. Existing residents will not doubt feel the loss of the view across the field, but loss of a view is not a material planning consideration and as stated above, the proposed dwellings are not situated so close to the existing as to create issues of overbearance.

- 10.48 It is noted that the previous refusal of planning permission F/YR16/0730/FDL included two reasons based on impact of noise from the proposed access on the amenity of 15 Eastalls Close and on 12 – 18 Bevills Close and impact on amenity to residents due to the vehicular access. It would have been difficult to substantiate concerns over noise had there been an appeal due to no objections at the time from FDC environmental health. Nevertheless, the previous scheme was for 62 dwellings whereas the current proposal is for 47 dwellings. The access as previously proposed was closer to the side elevation of 15 Eastalls Close. The current plan includes a footpath adjacent the boundary to No. 15 which is separated from the carriageway by green planted area. This is an improvement over the previously refused arrangement. The positioning of the access road in relation to 10, 20 and 12 – 16 Bevills Close and to 15 Eastalls Close is no different than many other comparable situations where there is a junction in an estate road layout. There will be traffic passing by these properties where currently there is none, but the access road is not considered to have an unacceptable relationship with the neighbouring properties. In addition, vehicles will not be travelling at speed here due to proximity to the junction and the kink in the new road layout.
- 10.49 Anglian Water has stated that dwellings placed within 15 metres of the sewage pumping station to the southwest (adjacent to the site off Eastmoor Lane) may be affected by issues of odour and nuisance from maintenance activity. The proposal provides for the open space area to be located adjacent to the pumping station and the closest proposed dwelling is more than 15 metres away.
- 10.50 The proposed emergency access through what is presently part of 44 Bevills Close, will lead to increased footfall past all of the dwellings in this part of Bevills Close where currently it is limited to those people who live and visit the dwellings in the cul-de-sac. This route is to be situated adjacent to 39 Bevills Close and a rebuilt new bungalow at 44. Although an emergency vehicular access, it will not be permanently open to vehicular traffic. There are two sets of lockable bollards proposed at either end of the access. However, the detailed design would be subject to condition if permission is granted. Whilst this will result in a very noticeable change over the current situation, it is considered that this is not a significant loss of residential amenity. The new bungalow in place of the existing at No. 44 will not have any adverse impact on the amenity of the neighbouring residents due to its scale, orientation and position of windows. The developer will need to liaise with the owner of No. 39 with regard to the demolition of half of the shared garage. Concerns have been raised about potential misuse of the footpath/cycleway by persons on motorbikes or scooters but this is not a reason to not allow a new footpath/cycleway. Efforts will be taken through the design of the barrier and signage (possible traffic regulation if adopted by CCC) to reduce the potential for misuse.

- 10.51 Once the development is completed, the development will not have a significant detrimental impact on neighbours in terms of noise, light pollution, loss of privacy or loss of light and therefore complies with policy LP16 in this regard.
- 10.52 Policy LP16 (h) requires that developments provide sufficient amenity space suitable to the type and amount of development proposed. This means that a minimum of a third of the plot curtilage should be set aside as private amenity space. The proposal complies with this policy. The size of rear private gardens is commensurate with the size of dwelling it serves. Distances between the new dwellings are also acceptable.

Infrastructure and Viability

- 10.53 Policy LP13 of the Local Plan states that planning permission will only be granted where there is sufficient infrastructure capacity to support the requirements arising from the proposed development. Developers will make direct provision or contribute towards local and strategic infrastructure required by the development either alone or cumulatively with other developments.
- 10.54 Policy LP13 is underpinned by the Developer Contributions SPD which sets out the Council's approach to seeking developer contributions.
- 10.55 The Local Plan CIL Viability Assessment published in December 2019 suggests that the current typical contribution of about £2000 per dwelling will continue if an affordable housing target of 20% is sought. Due to the difference in land values between the north and south of the district and also between greenfield and brownfield sites, achieving affordable housing and contributions on sites with lower values will be challenging.
- 10.56 The NPPG sets out the approach that should be taken towards viability. The weight to be given to a viability assessment submitted with an application is a matter for the decision maker. In decision making (and plan making), viability helps to strike a balance between the aspirations of developers and landowners in terms of returns against risk, and the aims of the planning system to secure maximum benefits in the public interest through granting of planning permission.
- 10.57 The viability assessment submitted with the application has been examined by the Council's Senior Planning Obligations Officer who concluded that the development could afford a total contribution of £72,000 and no affordable housing.
- 10.58 The land to the northern part of the site was offered as possible public open space or land that could be transferred to the County Council Education Authority as an extension of the Walden Primary School site. Discussions were held with the County Council and school who stated that they would like to take the land due to the small outside space associated with the school and difficulty in providing expansion. The case officer agreed that the land would best serve the local community as school land rather than public open space (especially as the south of the site also provides open space). A valuation report was submitted which values the potential school land at £93,500 and the cost of the works required to make the land suitable for sports/play use by the school at £42,500. Therefore, the total benefit being transferred by the applicant to the County Council is £136,000. The

Council's Senior Planning Obligations Officer has reviewed this report and agrees with its findings.

10.59 Therefore, both in terms of value and the transfer of land to education use, it is considered that this is the best that can be achieved from the site and that incorporation of this land into the school is the most beneficial outcome for the local community that can be achieved, given the viability constraints associated with the proposal.

Other Issues

10.60 Archaeology

The applicant has submitted a Written Scheme of Investigation which has been accepted by CCC Archaeology. A condition would need to be imposed requiring the WSI to be implemented in full.

Crime and Fire Safety

10.61 The Designing out Crime Officer for CCC Constabulary has requested a lighting scheme be conditioned and that garden gates be self-closing and lockable. These requests are considered to be reasonable and in compliance with policies LP16 and LP17 which require incorporation of safety measures to create safer environments and to deter crime. The provision of fire hydrants as requested by the Fire Service also meets this requirement.

10.62 Access through historic core of village/damage to trees and listed buildings (raised by objectors)

As set out in the above report, the site is separated from the historic core of the village by the existing Bevills Close estate. This development directly joined onto the historic core of the village. Only the very northwestern corner (the point) is adjacent to a corner of the Conservation Area. It is considered that the completed development preserves the character and appearance of the Conservation Area due to the separation from the conservation area boundary. Access to the site will result in additional traffic as well as construction traffic passing through the historic core and past listed buildings. The access roads are still public highway and so traffic has a right to use these unrestricted roads. By the nature of these roads, HGVs involved in the construction should not be travelling at speed. It would be difficult if not impossible to provide evidence that vehicles travelling to the site would cause damage to historic buildings and if this were to be a reason for refusal it would need to be substantiated at any appeal with evidence. This is unlikely to be the case.

10.63 It will set a precedent (raised by objectors)

If this application is approved, it does not set a precedent. This site is very different to other land in the vicinity and in the village and each application must be considered on its own merits.

10.64 There will be anti-social behaviour (raised by objectors)

This is unsubstantiated speculation.

10.65 Part of the site is FDC owned land (raised by objectors and FDC Estates)

Any discussions or considerations concerning FDC owned land are for the developer to have with FDC Estates outside of the planning process and this has no bearing on the recommendation being made.

10.66 Devaluation of property (raised by objectors)

This is specifically excluded from being a material planning consideration

10.67 This is a “done deal” etc (raised by objectors)

The application has been assessed against the policies of the development plan and any other material considerations. Had, for instance, there been irrevocable issues with drainage or highways, the application would not have been recommended for approval. Whilst the case officer notes the genuine concerns raised by objectors, on matters of a technical nature, the advice of statutory consultees carries significant weight. On other matters such as design and impact on neighbours, the report above demonstrates that these more subjective issues have been carefully considered.

10.68 Previous Refusals

Planning history is a material consideration in determining a planning application, especially where it is recent, and the previous application(s) were considered against the prevailing development plan policy. As set out above, applications were refused in 2015 and 2016 for 66 and 62 dwellings respectively. Reasons for refusal were based on the following matters;

- Village thresholds
- Lack of capacity at Doddington Waste Water facility for treatment of foul sewage
- Impact on amenity of existing residents from the proposed vehicular access and lack of alternative parking for occupiers of 12 – 18 Bevills Close
- Impact of noise from access on 12 – 18 Bevills Close and 15 Eastalls Close
- Development of 62 dwellings is not small scale and will have a cumulative detrimental effect on the neighbouring housing estate through vehicle movements into the village

The majority of these issues have been considered in the main body of the report above, however, in summary, the response to these issues as follows;

- Village thresholds and requirement for evidence of local support as set out in policy LP12: Appeal inspectors have criticised this element of policy LP12 stating that planning is based upon land use principles and development would not come forward if acceptance were based upon local support. Notwithstanding this, the current proposal is now 15 dwellings less than the proposal refused in 2016.

- Lack of capacity at Doddington Waste Water Facility: this is not supported by Anglian Water who have not objected to the proposal.
- Impact on neighbouring residents and lack of parking for 12 – 18 Bevills Close – The proposal will result in a new street in this location where there is presently a turning head and no through traffic: This will of course be noticeable but is not unusual where new developments are accessed through existing areas. The new road provides adequate separation from adjacent dwellings. The allocated parking for 12 – 18 Bevills Close is retained. The existing highway cannot be considered to comprise parking for existing residents (as confirmed by the LHA). No objections concerning noise nuisance have been raised by the Environmental Health team.
- Impact of noise from the access on 12 – 18 Bevills Close and 15 Eastalls Close: response is as above. The current proposal moves the access and path further away from the side of 15 Eastalls Close than the previous refused scheme.
- 62 dwelling scheme is not small scale: the current proposal for 47 dwellings is considered to be small scale given the settlement hierarchy and the scale of development compared to the rest of Doddington. No objections from the LHA or EHO concerning vehicle movements through village. It is not considered that adequate evidence could be provided to substantiate this reason for refusal at appeal if the current proposal were refused for this reason. In addition, the current proposal is 15 dwellings less than the previous refused scheme.

10.69 The other issues raised by objectors have been covered in the preceding parts of this report.

Conditions

10.70 From 1 October 2018 section 100ZA(5) of the Town and Country Planning Act 1990 provides that planning permission for the development of land may not be granted subject to a pre-commencement condition without the written agreement of the applicant to the terms of the condition (except in the circumstances set out in the Town and Country Planning (Pre-commencement Conditions) Regulations 2018).

10.71 The applicant was sent a copy of the draft pre-commencement conditions set out below and has agreed to them.

11 CONCLUSIONS AND PLANNING BALANCE

11.1 Doddington is a growth village (policy LP3 of the Fenland Local Plan) within the settlement hierarchy where small village extensions will be appropriate. LP3 does not define what small means, however policy LP4 states that large scale housing of 250 dwellings or more should be directed to market towns. Taking into account the shape and size of the village and the proposed 47 dwellings, it is considered that the scale of development and location of the site are acceptable in principle and accord with policies LP3, LP4 and LP12 of the Local Plan.

11.2 The key issues (other than principle) in relation to this site are access provision and drainage. The statutory consultees i.e. the highway authority and the lead local flood authority have raised no objections to the proposals subject to conditions. Similarly Anglian Water confirms that there is adequate capacity in the sewerage

network to accommodate the development. The proposals demonstrate that adequate vehicular and pedestrian access is provided, and that surface water and foul sewage can be satisfactorily drained from the site. It is considered that in terms of highways and drainage issues, the proposal is acceptable and complies with policies LP12, LP14 and LP15 of the local plan.

- 11.3 With regard to other detailed matters including biodiversity, layout/appearance/design, impact on neighbour and future occupier amenity etc, the proposal is considered acceptable subject to conditions. In particular, the Council's Wildlife Officer comments that it will be easy to ensure that the site provides for a net gain in biodiversity.
- 11.4 The proposal is unable to meet the requirements for affordable housing set out in local plan policy LP5 together with the infrastructure requirements to serve the development, which previously the local planning authority has accepted will be in the order of £2000 per dwelling (due to viability issues within the district). The applicant has submitted a viability assessment which concludes that the development can afford contributions to a value of £72,000. In lieu of this, a serviced land transfer has been negotiated which has an accepted value of £136,000. The likelihood of the school being able to obtain more land is low and so it is considered that this provision carries significant weight in favour of the scheme. The viability assessment must carry weight in favour of the scheme due to the acknowledged issues around viability within Fenland and because the Council's Senior Planning Obligations Officer has found this particular viability appraisal to be robust.
- 11.5 The latest Five-Year Housing Land Supply report (September 2021) shows that the Council currently has a 6.69 years supply of deliverable sites. The details of the supply can be found within the report which includes details of the calculations for the Local Housing Need figure. This indicates that 517 dwellings per annum will be required. An updated 5 Year Housing Land Supply report is due to be published in Autumn 2022 which will include calculations based on the most up-to-date available data. As such the tilted balance set out within the NPPF is not engaged. However, the existence of a 5-year supply is not reason to refuse acceptable residential developments coming forward. The NPPF sets out at paragraph 60 that the government's objective is to significantly boost the supply of homes. The lack of provision of affordable housing counts against the scheme but the provision of a mix of 47 market dwellings counts in favour of the scheme and balancing the two carries moderate weight in favour of the scheme.
- 11.6 In other respects, the proposal complies with the provisions of the development plan. It is acknowledged that the construction traffic and build programme generally will have a negative impact on nearby residents, but this is a temporary impact and not reason to refuse the application. This issue can only carry limited weight and can be tempered to degree by a CMP condition.
- 11.7 In conclusion, the application is considered to be acceptable subject to the imposition of conditions and the entering into of a S106 agreement to transfer the land for the school to the County Council.

12 RECOMMENDATION

- (i) Grant subject to the conditions listed below and completion of a S106 Agreement,
- (ii) Delegate to Head of Planning to refuse planning permission if there is insufficient progress towards the completion of the legal agreement,
- (iii) Delegate authority to Head of Planning to impose any other conditions which may be required;

1	<p>The development permitted shall be begun before the expiration of 3 years from the date of this permission.</p> <p>Reason - To ensure compliance with Section 51 of the Planning and Compulsory Purchase Act 2004.</p>
2	<p>No development shall take place until detailed drawings, specifications and phasing plans for the roads and footpaths including the emergency access have been submitted to and approved in writing by the local planning authority. The roads other than private drives shall be designed to an adoptable standard. The details for the emergency access shall include details of the lockable bollards and/or alternative/additional measures to deter this access from being used by vehicular traffic except in an emergency situation. The phasing plan shall include details of the timing of delivery of the roads and footpath/cycleways alongside the delivery of the dwellings. The development shall only take place in accordance with the approved plans and details. The approved lockable bollards or alternative/additional measures to be installed at the emergency access shall be retained and maintained as such in perpetuity.</p> <p>Reason: In the interests of ensuring that the development is served by sustainable access in accordance with policy LP15.</p>
3	<p>Prior to the commencement of development, a Construction Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The CMP shall contain as a minimum:-</p> <ul style="list-style-type: none"> - Hours of Site working and deliveries - Parking provision for site workers - Layout of site compound and materials store - Dust management measures - Measures to minimise mud from being taken onto the highway from the site - Name and contact details for on site person responsible for compliance with this CMP (including an out of hours contact) - Details of temporary fencing during construction - Measures to prevent avoidable noise nuisance to neighbours e.g. no radios or amplified music <p>The approved CMP shall be implemented in full throughout the construction period.</p>

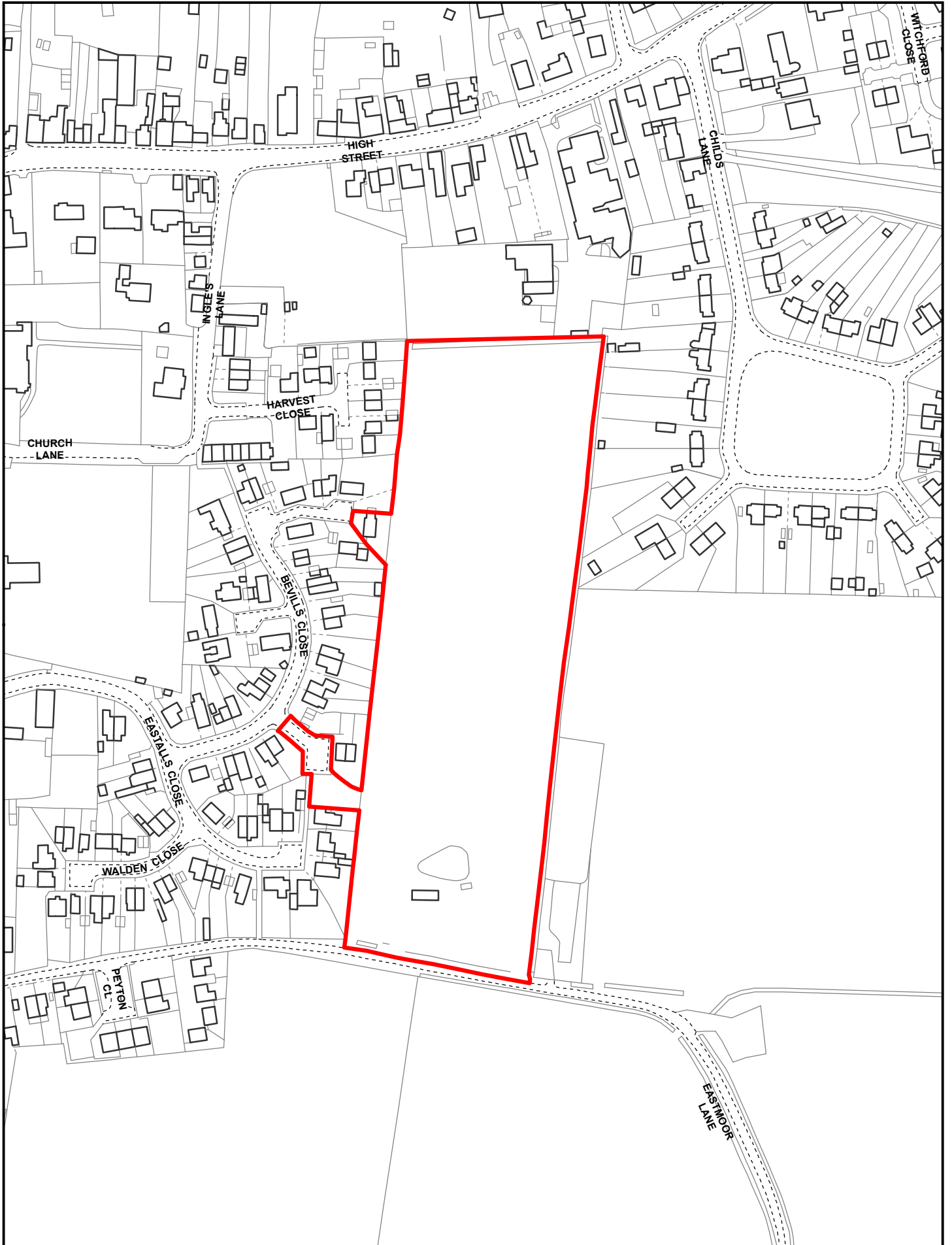
	Reason: In the interest of neighbour amenity in accordance with policy LP16 of the Fenland Local Plan
4	<p>Prior to the commencement of the development and clearance of the site, details of tree protection measures for all trees to be retained (including trees where the canopy overhangs the site boundary from adjacent land) shall be submitted to and approved in writing by the local planning authority. Such tree protection measures shall accord with BS 5837:2012. The approved tree protection measures shall be put in place prior to any site clearance, materials being brought onto site or any development commencing and shall remain fully in place until the development is completed. Where development is completed in relation to a particular tree i.e. no further development will take place in the vicinity of the tree then that particular tree protection measure may be removed.</p> <p>Reason: In the interests of the visual appearance of the development and protection of retained trees/existing biodiversity in accordance with policies LP16 and LP19 of the Fenland Local Plan.</p>
5	<p>Further to the submitted Phase 1 Desk Study report by Geodyne Ref: D39218, no development shall take place until a programme of ground gas monitoring has been undertaken to confirm the presence of ground gas. In addition an asbestos survey shall be carried out to identify any potential presence of asbestos within the dilapidated building on site. In addition, an exploratory ground investigation shall be carried out to further investigate the potential for risk of contaminants at the site. This shall include as minimum programme of exploratory hole undertaking and the subsequent testing of soil samples taken from relevant parts of the site, a certified geotechnical soil analysis, the asbestos survey and installation of ground gas monitoring points and a programme of ground gas monitoring to determine the extent of gas presence.</p> <p>No development shall take place until the results of the above investigations have been analysed and written up into a report by suitably qualified persons and submitted to and approved in writing by the local planning authority. The report shall include any necessary remediation measures to make the development safe and a timetable for undertaking such measures which shall be implemented in full. No dwelling shall be occupied until a verification report written by a suitably qualified person, which verifies that approved remediation measures have been fully implemented has been submitted to and approved in writing by the local planning authority. If ground condition circumstances suit, this may be able to be done on a phased basis.</p> <p>Reason: In the interests of public safety and prevention of pollution to the environment in accordance with policy LP16 of the Fenland Local Plan</p>
6	Prior to the commencement of the development, the site investigation

	<p>shall be carried out in complete accordance with the approved Written Scheme of Investigation dated April 2022 (version 2 on page 17 of the report), prepared by APS. Thereafter, the development shall only take place in accordance with the remainder of the approved WSI including the post excavation report, report deposition, archive and publication, the details of which shall have been submitted to and approved in writing by the local planning authority prior to the completion of the development or the occupation of the 40th dwelling, whichever is the sooner.</p> <p>Reason: In the interests of recording and preserving any historic assets that may otherwise be destroyed by the development in accordance with policy LP18 of the Fenland Local Plan and the NPPF.</p>
7	<p>No laying of services, creation of hard surfaces or erection of a building shall commence until a detailed design of the surface water drainage of the site has been submitted to and approved in writing by the Local Planning Authority. Those elements of the surface water drainage system not adopted by a statutory undertaker shall thereafter be maintained and managed in accordance with the approved management and maintenance plan. The scheme shall be based upon the principles within the agreed Flood Risk Assessment & Drainage Strategy report prepared by Inspire (ref: AHL-1549-01-FRA-001-P3) dated July 2022 and shall also include:</p> <ul style="list-style-type: none"> a) Full calculations detailing the existing surface water runoff rates for the QBAR, 3.3% Annual Exceedance Probability (AEP) (1 in 30) and 1% AEP (1 in 100) storm events; b) Full results of the proposed drainage system modelling in the above-referenced storm events (as well as 1% AEP plus climate change), inclusive of all collection, conveyance, storage, flow control and disposal elements and including an allowance for urban creep, together with an assessment of system performance; c) Detailed drawings of the entire proposed surface water drainage system, attenuation and flow control measures, including levels, gradients, dimensions and pipe reference numbers, designed to accord with the CIRIA C753 SuDS Manual (or any equivalent guidance that may supersede or replace it); d) Full detail on SuDS proposals (including location, type, size, depths, side slopes and cross sections); e) Temporary storage facilities if the development is to be phased; f) A timetable for implementation if the development is to be phased; g) Details of overland flood flow routes in the event of system exceedance, with demonstration that such flows can be appropriately managed on site without increasing flood risk to occupants; h) Demonstration that the surface water drainage of the site is in accordance with DEFRA non-statutory technical standards for sustainable drainage systems; i) Full details of the maintenance/adoption of the surface water drainage system; j) Measures taken to prevent pollution of the receiving groundwater and/or surface water The drainage scheme must adhere to the hierarchy of drainage options as outlined in the NPPF and PPG

	<p>Reason: To ensure that the proposed development can be adequately drained and to ensure that there is no increased flood risk on or off site resulting from the proposed development and to ensure that the principles of sustainable drainage can be incorporated into the development, noting that initial preparatory and/or construction works may compromise the ability to mitigate harmful impacts</p>
8	<p>No development shall take place above slab level until full details of all hard and soft landscaping and biodiversity enhancements have been submitted to and approved in writing by the local planning authority. The details shall include but not be limited to the following;</p> <ul style="list-style-type: none"> -All existing trees to be retained and removed -All new soft landscaping, including species, sizes, numbers and spacing of plants - Identification on a drawing of all trees and plants which contribute towards biodiversity enhancement and towards the net gain in biodiversity - Details of all other biodiversity enhancements including specifications and locations based on the approved Biodiversity Plan and Construction Ecological Management Plan (drawing 53-BDP-01 Rev A) together with details of evidence of how the stated biodiversity net gain of 11 percent across the site will be achieved and how the condition level, described using Biodiversity Metric 3.1 calculations (or subsequent variations) will be reached through on-going management of the site. - Details of all hard surfaced areas -Timetable for provision <p>The development shall only take place in complete accordance with the approved details.</p> <p>Any plants or trees that contribute towards the biodiversity enhancement of the site shall be retained and if any die, become diseased or are removed or uprooted, shall be replaced in the next planting season with a plant of similar species, size and biodiversity value. All other biodiversity enhancements (e.g., bird boxes) shall be retained and maintained in perpetuity.</p> <p>Reason: In the interests of the visual appearance of the development and to achieve a net gain in biodiversity in accordance with policies LP12, LP16 and LP19 of the Fenland Local Plan and the NPPF.</p>
9	<p>No development above slab level shall take place until a full specification for the approved play area has been submitted to and approved in writing by the local planning authority. The details shall include but not be limited to;</p> <ul style="list-style-type: none"> -Detailed drawings to scale or manufacturers specifications of all play equipment -Means of enclosure -Surfacing -A dog waste bin (to be located outside of the play area within the public open space) -Timetable for provision

	<p>The development shall only take place in complete accordance with the approved details and timetable for provision.</p> <p>Reason: In accordance with the submitted site plan and in the interests of public amenity and health and well-being in accordance with policies LP2 and LP16 of the Fenland Local Plan</p>
10	<p>Prior to development above slab level taking place and concurrently with the details to be submitted in respect of the discharge conditions 8 and 9, details of the management arrangements for all areas that will fall outside the curtilage of a dwelling, the adopted highway and other land to be transferred to the County Council shall be submitted to and approved in writing by the local planning authority. The details shall include but not be limited to the following;</p> <ul style="list-style-type: none"> -Name and full contact details of the management company -Drawing showing the areas to be managed by the management company -Maintenance details and schedules for all hard and soft landscaping, the play area, dog bin, lighting and biodiversity enhancements that fall within the management company area of maintenance <p>All such areas shall be maintained by the developer until such time as the management company takes over responsibility for these areas. The developer shall provide written notification to the local planning authority that the management company has taken over the maintenance of these areas within 5 working days of this event taking place.</p> <p>Reason: To ensure that all landscaped areas, play area, biodiversity enhancements and other items as approved continue to be maintained in the interests of the visual appearance of the area, residential amenity, health and well-being of residents and biodiversity in accordance with policies LP2, LP12, LP16 and LP19 of the Fenland Local Plan</p>
11	<p>Prior to the commencement of development above slab level details of all external lighting shall be submitted to and approved in writing by the local planning authority. This shall include details of lighting columns to serve both the vehicular access to be adopted and private drives. The lighting shall be provided prior to the first occupation of the development areas which it serves and retained as such thereafter.</p> <p>Reason: In the interests of public safety and the reduction of crime in accordance with policies LP16 and LP17 of the Fenland Local Plan</p>
12	<p>Prior to the commencement of the development a scheme and timetable for the provision of fire hydrants shall be submitted to, and agreed in writing by, the Local Planning Authority in consultation with the Chief Fire Officer and provision of the fire hydrants shall be made in accordance with the scheme and timetable.</p> <p>Reason - To ensure a satisfactory form of development in the interest of community safety in accordance with policies LP16 and LP17 of the</p>

	Fenland Local Plan.
13	<p>All boundary screening/treatment shall be provided prior to the occupation of the dwelling or area it serves in accordance with the approved Boundary Treatment Plan 53-BT-01 Rev G. All gates serving private rear gardens to dwellings shall be self-closing and lockable, the details of which shall be submitted to and approved in writing by the local planning authority prior to the first occupation of any dwelling.</p> <p>Reason: In the interests of visual appearance, privacy and security in accordance with policy LP16 of the Fenland Local Plan.</p>
14	<p>The external materials to be used for each dwelling shall accord with the details shown on the submitted Materials Schedule Layout drawing 53-MSL-01 Rev D.</p> <p>Reason: To achieve a satisfactory external appearance in accordance with policy LP16 of the Fenland Local Plan</p>
15	<p>No dwelling shall be occupied until the car parking and any turning area to serve that dwelling has been provided in accordance with the approved Site Layout Plan 53-SL-01 Rev I.</p> <p>Reason: To ensure that adequate parking provision is made in accordance with policy LP15 and Appendix A of the Fenland Local Plan.</p>
16	Approved plans



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F/YR21/1072/FDC

Scale = 1:2,500



HOUSE TYPES



DEE - 2b 4p



WARWICK - 2b 4p



AIRE - 3b 5p



NENE - 3b 5p



LOCK - 3b 5p



MERE - 3b 5p



RIBBLE - 4b 7p



BALMORAL - 4b 7p



HUMBER - 4b 7p



□	2000/2002	CEILING	CEILING	CEILING	CEILING	CEILING	CEILING	CEILING	CEILING
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□	0000/2002	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE
□	0000/2002	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE	RECYCLE
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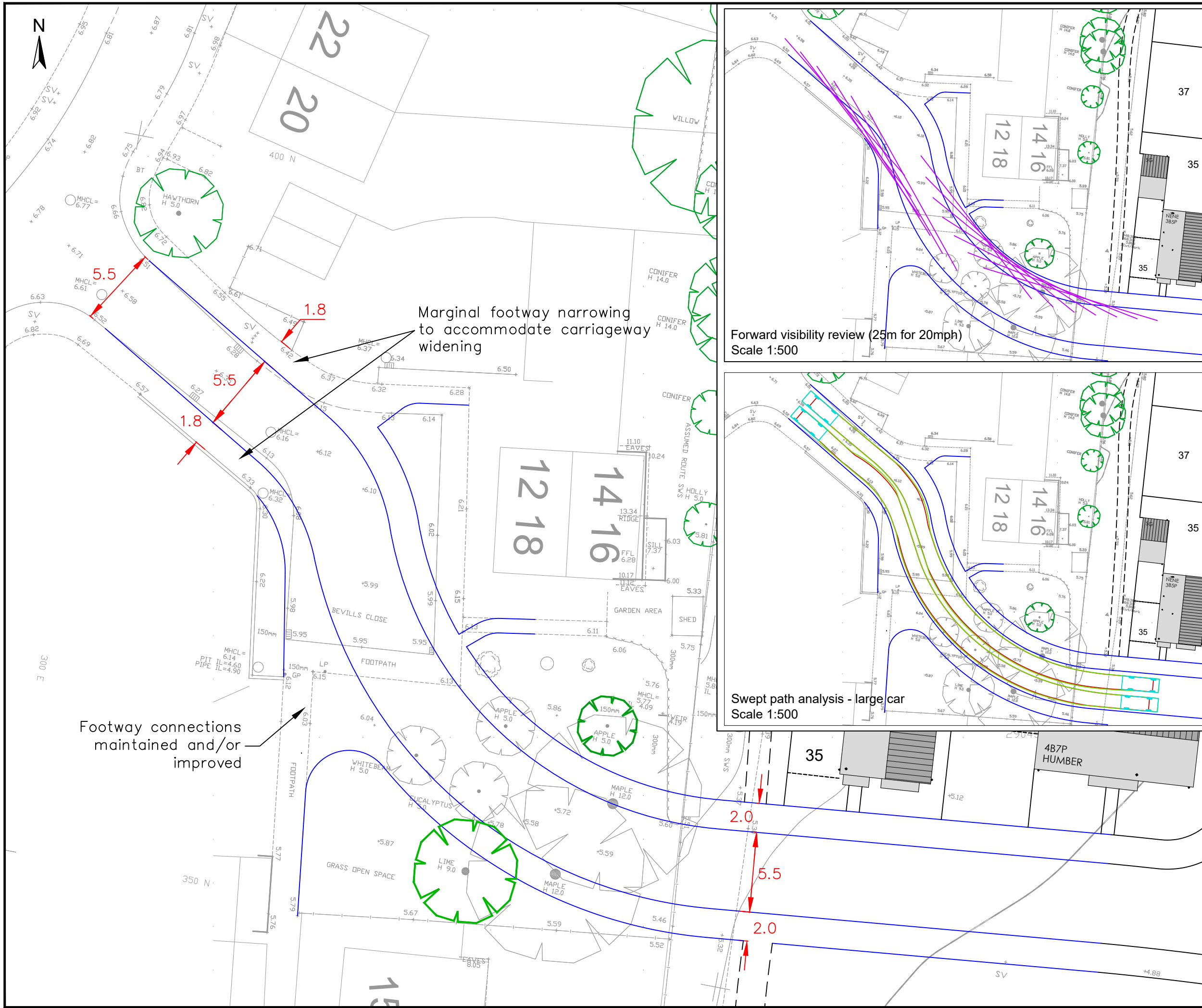
ASHWOOD HOMES
 1 Goodison Road, Lincs Gateway Business Park,
 Spalding, Lincs, PE12 6FY T: 01406 490590

PROJECT: Proposed Residential Development at Bevills Close Doddington

DRAWING: Site Layout Colour

SCALE: 1:100 A1 DRAWN: MCD
 DATE: 01/02/2022 REVISION: □

DRAWING No: 53-SL-02



Marginal footway narrowing to accommodate carriageway widening

Footway connections maintained and/or improved

Forward visibility review (25m for 20mph)
Scale 1:500

Swept path analysis - large car
Scale 1:500

Rev.	Date	Amendment	Drawn	Chkd.	Appd.
A	30.11.21	Revised alignment and analysis	IW		



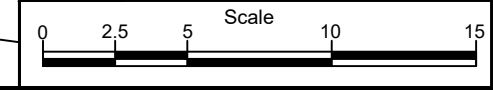
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Web: www.rsk.co.uk

Client
Ashwood Homes

Project Title
**Bevills Close
Doddington**

Drawing Title
**Proposed Site
Access Arrangements:
South Stub - Main Access**

Drawn	Date	Checked	Date	Approved	Date
MQ	MAY21	IW	MAY21		
Scale	1:250	Orig Size	A3	Dimensions	METRES
Project No.	663069		Drawing File		
Drawing No.	663069-10-02		Rev.	A	





Walden Primary School

KEY

- SITE BOUNDARY
- SITE WELFARE
SITE OFFICE & CANTEEN
- SILOS
STORAGE CONTAINERS
- COMPOUND STORAGE
- SITE INGRESS & EGRESS
TO COMPOUND & SITE PARKING
- TRAFFIC IN
- TRAFFIC OUT



SITE WORKING HOURS

To be 7.30am - 5.00pm Monday to Friday. In the event that Saturday working becomes necessary this is to be restricted to the hours of 8.00am - 1.00pm.

Materials suppliers are to be instructed not to arrive to site prior 7.30am to avoid disturbance to neighbors or potential highway obstruction.

Control of dust

Where dust issues are likely to occur, for example when cutting and excavating, dust suppression and use of RPE (respiratory protection equipment), and where appropriate wetting practices should be implemented.

The condition of site haul routes should be monitored. Mud is not to be taken onto public roads. Road sweeping and road wetting to be implemented as required.

Noise Management

The site manager should monitor noise pollution across their site at all times.

Where practically possible, 'quite' equipment should be used. Machinery and equipment should only be used for the required time.

Vehicles should not be revved and engines should not be left running when not required to be in operation.

Rev.	Date	Description
A	01.08.2022	UPDATED TO NEW LAYOUT

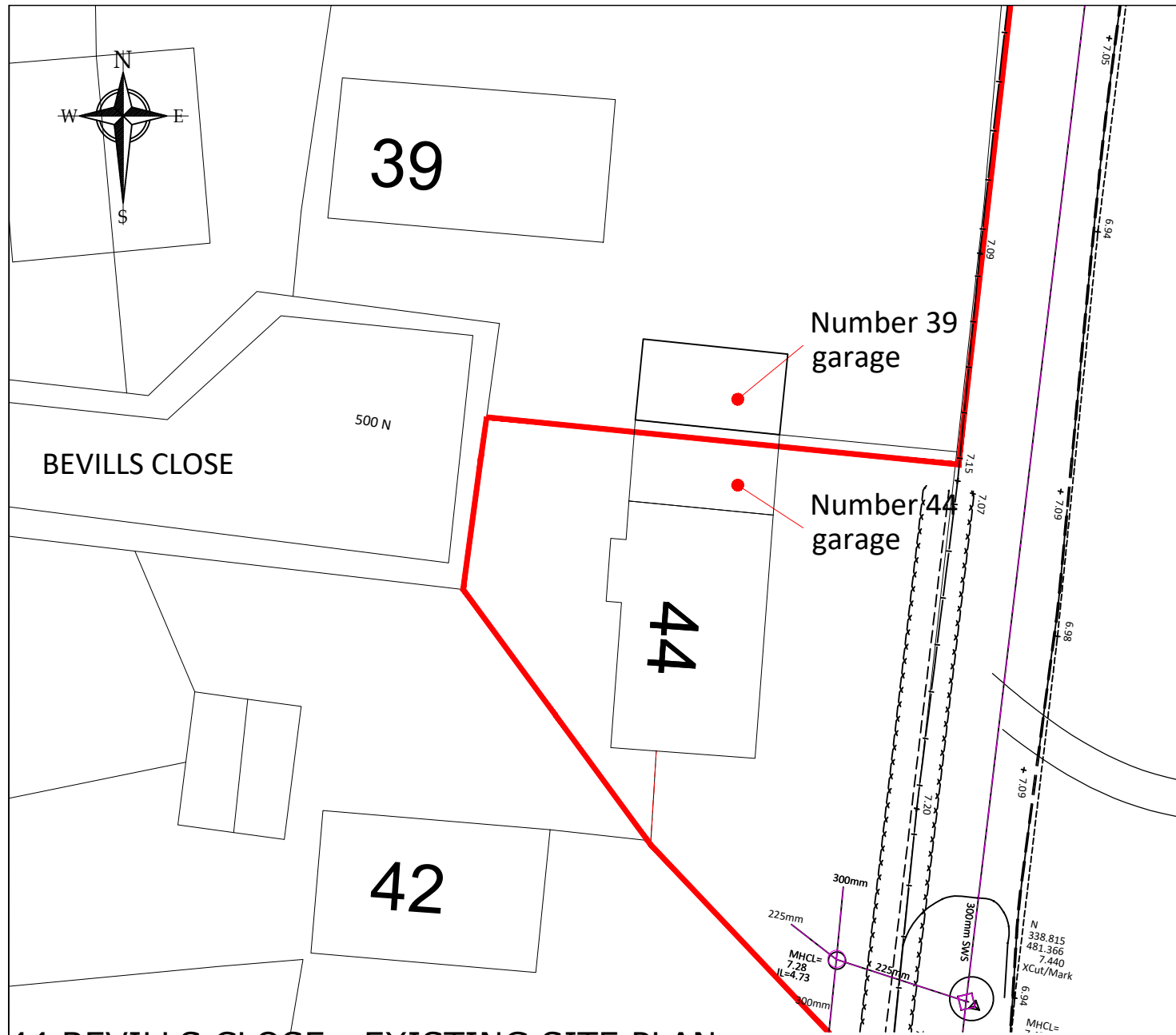


PROJECT: Proposed Residential Development at Bevills Close Doddington

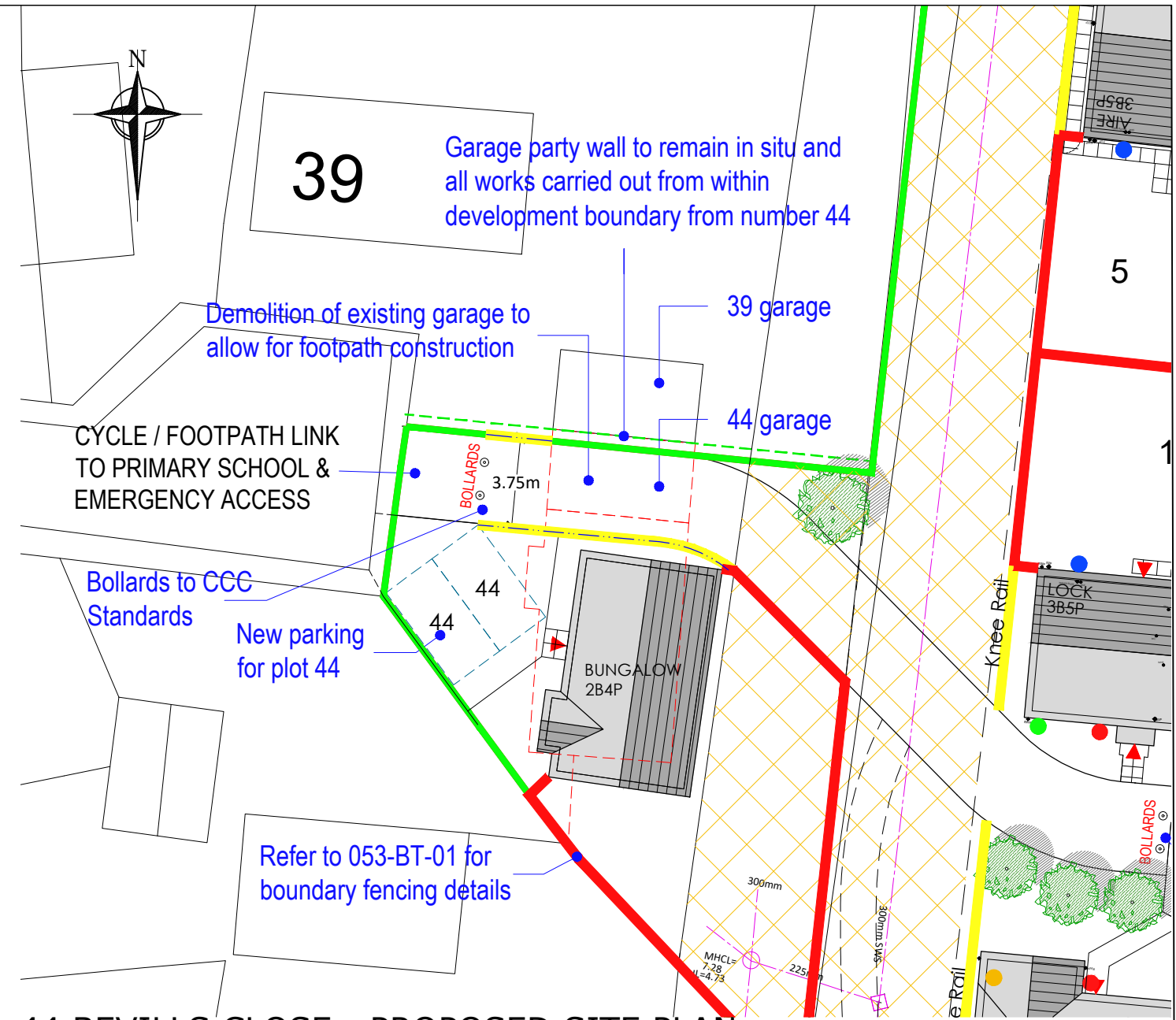
DRAWING: Construction and Traffic Management Plan

SCALE: 1:500@A1 DRAWN: MII
DATE: DEC.2021 REVISION: A

DRAWING No: 53-CMP-01



44 BEVILLS CLOSE - EXISTING SITE PLAN
Scale 1:250



44 BEVILLS CLOSE - PROPOSED SITE PLAN
Scale 1:250

44 Bevills Close to be carefully dismantled and demolished. Plot 44 garage to be removed from within development boundary retaining party wall to Plot 39 Garage.

Proposed 2 Bed 4 Person Bungalow to be built with 2 front parking spaces.

3.75m Cycle / Footpath Link to be constructed between plots 39 and 44 to link the development to Bevills Close. This is to be constructed to Cambridgeshire County Council Highway standards and will act as an emergency vehicular access only. Removable bollards will be placed at the front to prevent unauthorised vehicles accessing the link. For normal situations, this will serve as a pedestrian and cycle link.

Rev.	Date	Description
A	23.05.2022	Bollards moved back, knee rail fencing added

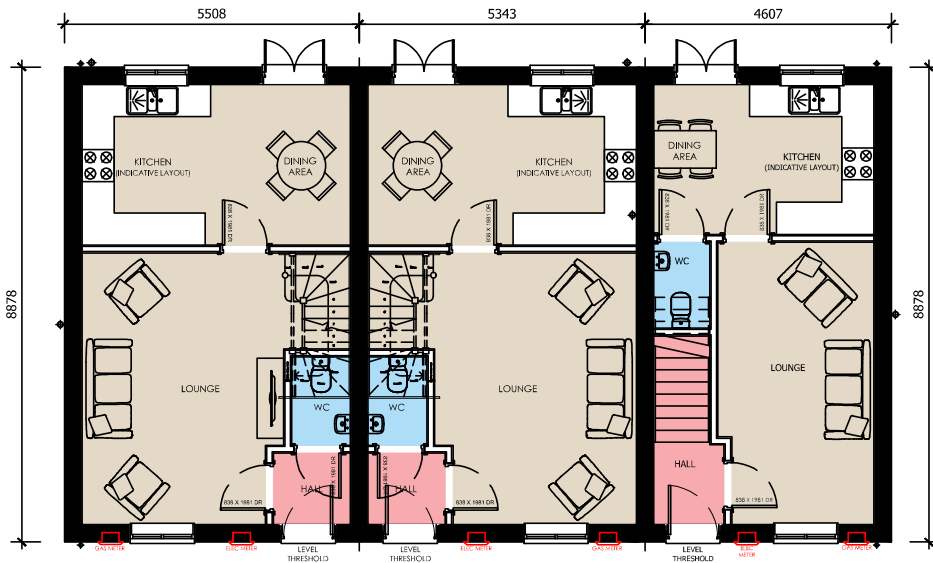
ASHWOOD HOMES
1 Goodison Road, Lincs Gateway Business Park, Spalding, Lincs, PE12 6FY T: 01406 490590

PROJECT: Proposed Residential Development at Bevills Close, Doddington

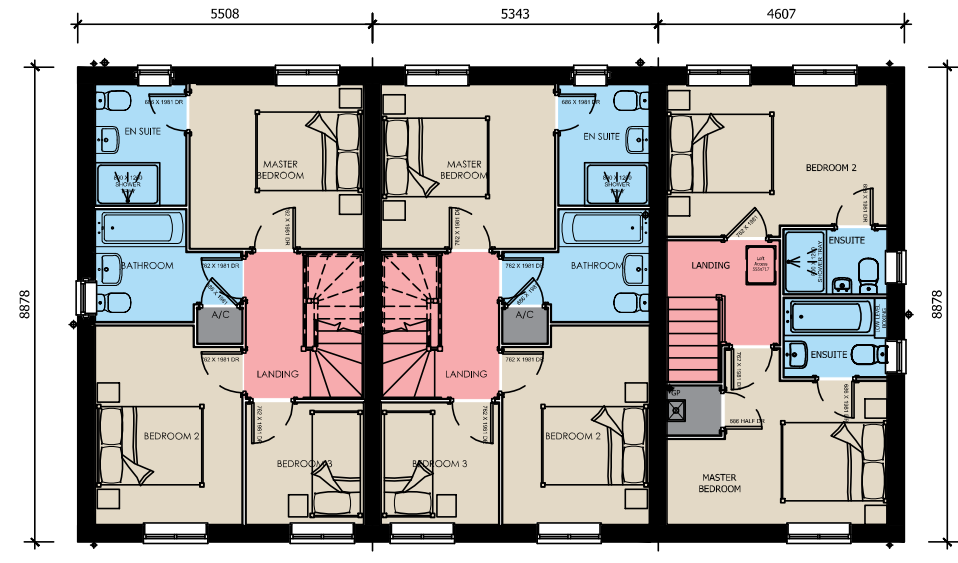
DRAWING: 44 Bevills Close Existing and Proposed Plans

SCALE: 1:250@A3 DRAWN: GB
DATE: MAY.2022 REVISION: A

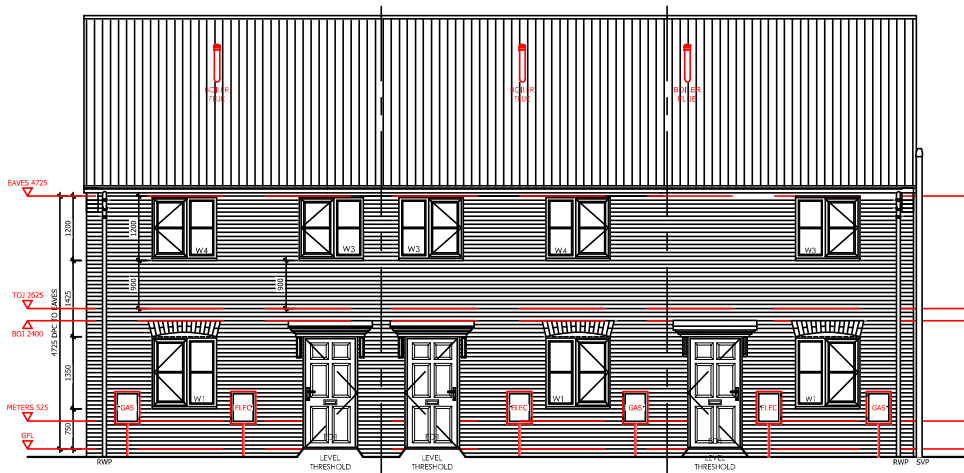
DRAWING No: 053-BV-01



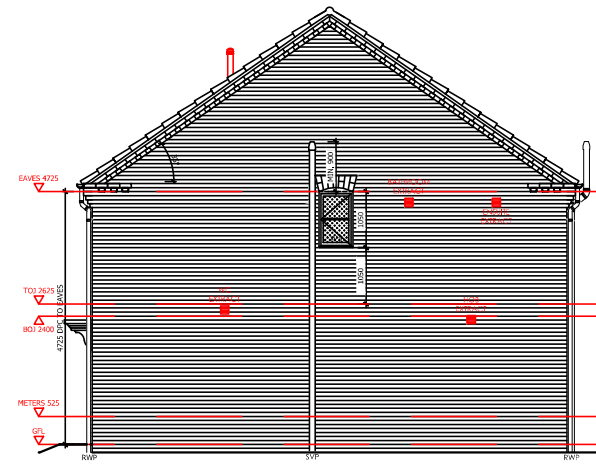
GROUND FLOOR



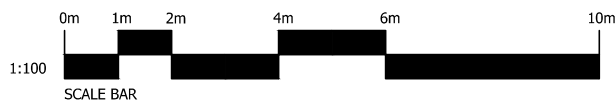
FIRST FLOOR



FRONT ELEVATION



SIDE ELEVATION



	HOUSE TYPE:	Aire - Aire - Dee (902 / 902 / 732 FT ²)
	DRAWING TITLE:	DESIGN SHEET
	DRAWING NUMBER:	Aire/Aire/Dee-001
	1 Goodison Road, Lincs Gateway Business Park, Spalding, Lincs, PE12 6FY T: 01406 490990	



FRONT ELEVATION



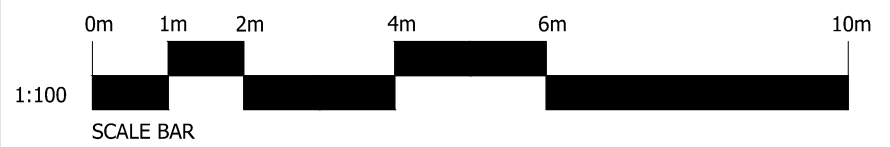
SIDE ELEVATION




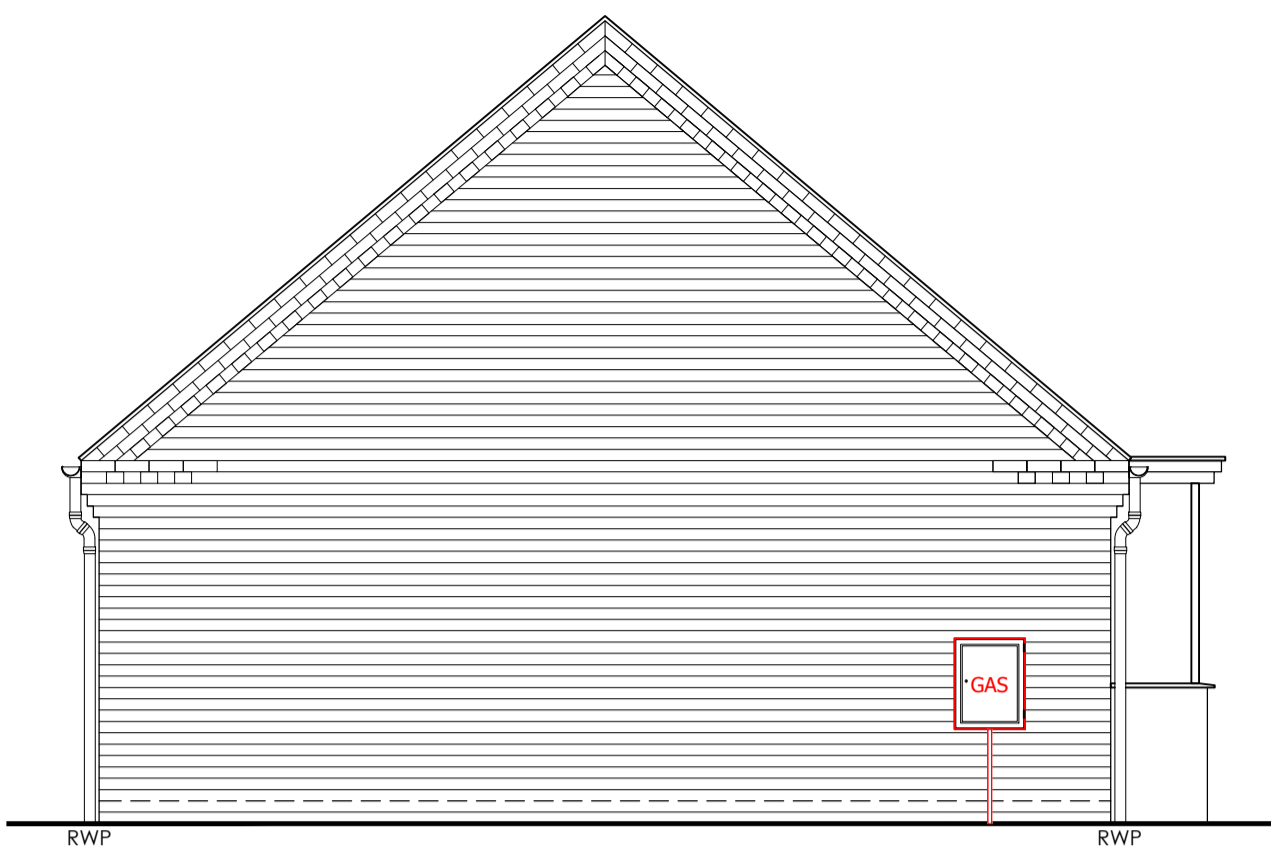
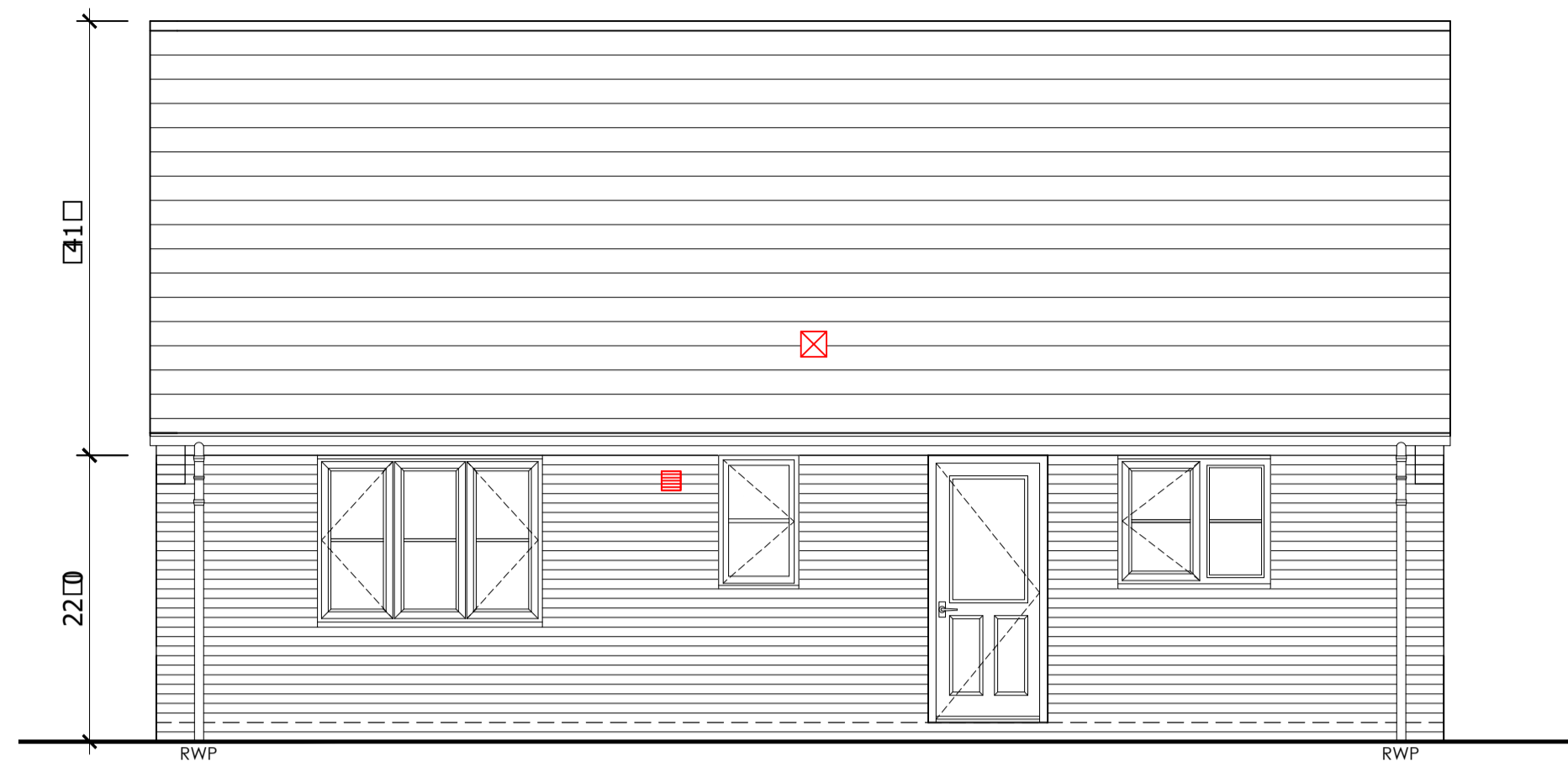
REAR ELEVATION



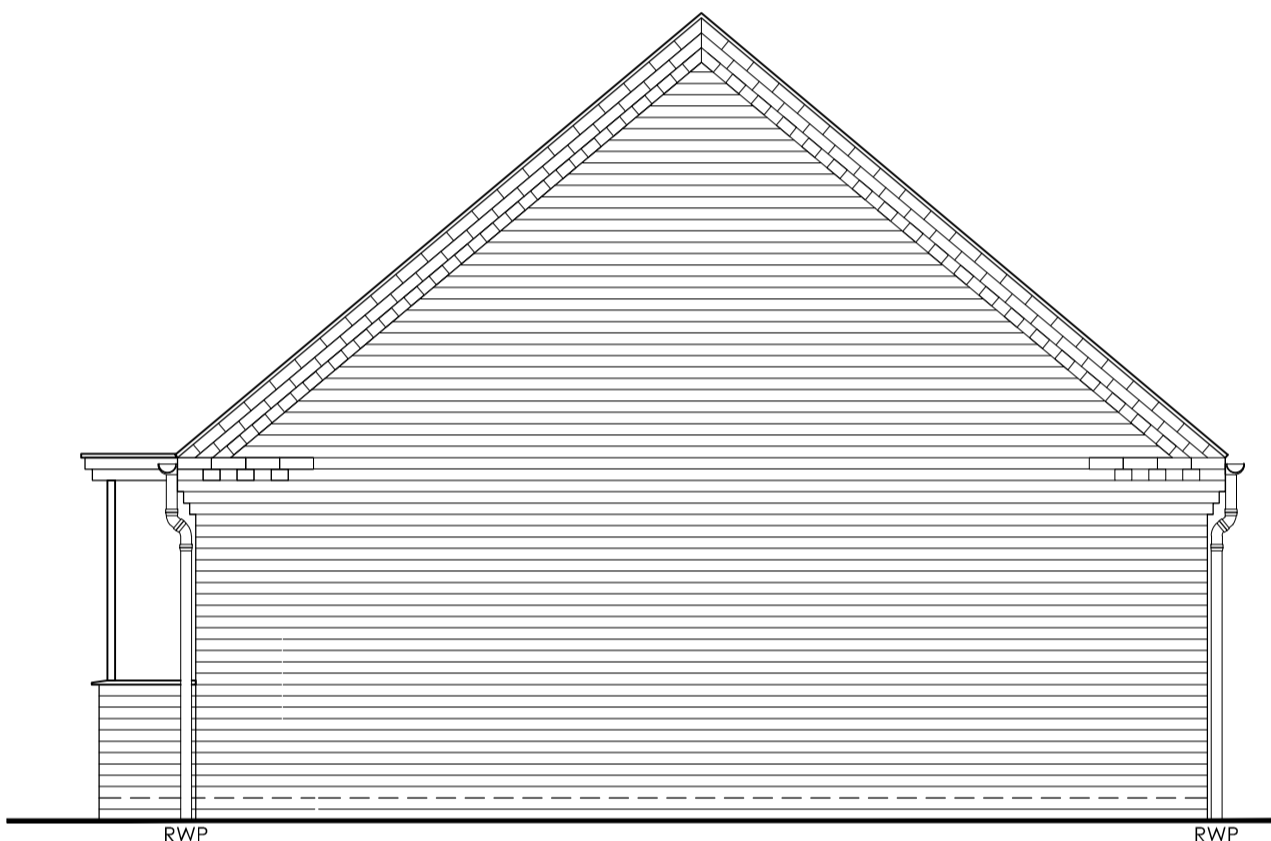
SIDE ELEVATION



 <p>1 Goodison Road, Lincs Gateway Business Park, Spalding, Lincs, PE12 6FY T: 01406 490590</p>	HOUSE TYPE:	HUMBER (1578 FT ²)
	DRAWING TITLE:	DESIGN SHEET
	DRAWING NUMBER:	HUMBER-002

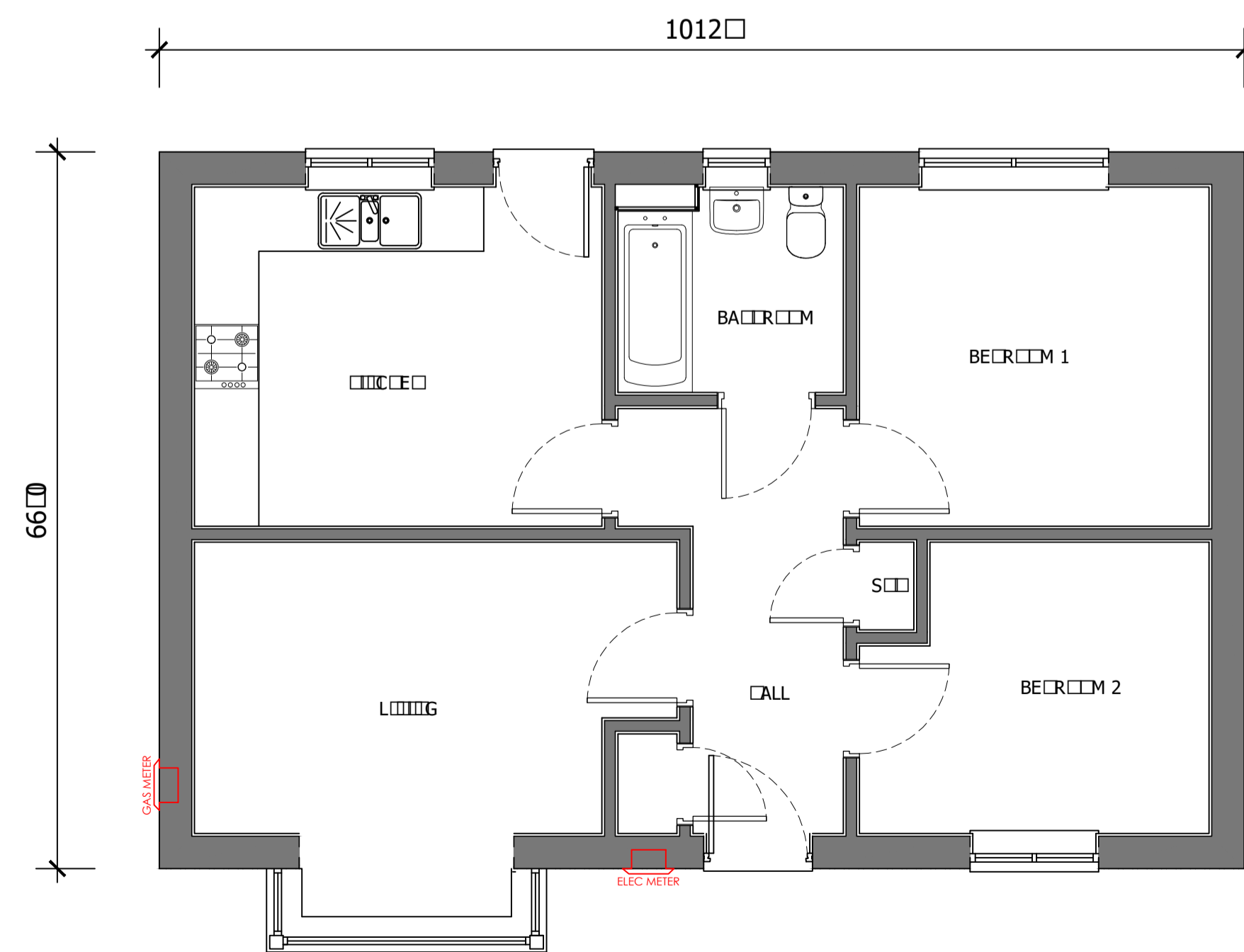


PROPOSED SIDE ELEVATION



PROPOSED SIDE ELEVATION

PROPOSED FRONT ELEVATION



PROPOSED FLOOR PLAN



1 Goodison Road, Lincs Gateway Business Park,
Spalding, Lincs, PE12 6FY T: 01406 490590

PROJECT:
Proposed Residential
Development at
Bevills Close, Doddington

DRAWING:
No 44 Bevills Close Proposed
Floor Plan and Elevations

SCALE: 1:100 DRAWN: M
DATE: 01/2022 REVISION: 0

DRAWING No: 53-PD-01

